

## **PLANNING APPLICATION NUMBER:P10/1429**

Type of approval sought	Full Planning Permission
Ward	Wollaston & Stourbridge Town
Applicant	TESCO STORES LIMITED
Location:	<b>THE CROWN CENTRE, STOURBRIDGE, WEST MIDLANDS, DY8</b>
Proposal	<b>DEMOLITION OF EXISTING CROWN CENTRE AND MULTI-STOREY CAR PARK AND CONSTRUCTION OF NEW LARGE RETAIL STORE (A1), SMALL RETAIL UNITS (A1-A5) AND OFFICES (B1) WITH ASSOCIATED HIGHWAY WORKS, SERVICING AND CAR PARKING. PUBLIC REALM WORKS (INCLUDING TOWN SQUARE), LANDSCAPE WORKS, ALTERATIONS TO TOWN HALL AND LIBRARY (INCLUDING TEMPORARY ACCESS).</b>
Recommendation Summary:	<b>APPROVE SUBJECT TO CONDITIONS</b>

### **SITE AND SURROUNDINGS**

1. The application site extends to 2.39 hectares of land located on the western edge of Stourbridge town centre. This distinctive site forms part of the land enclosed by the main Stourbridge ring road. It incorporates the Crown Centre and Bell Street multi-storey car park. The buildings are mainly constructed in red/brown brick with access stairs and service areas, the associated car parks are in a very traditional concrete format set over various levels. The particular site is bisected by a section of pedestrianised routes and open highway (New Street), also adjacent to Bath Road (Ring Road) to the west, Crown Lane to the north and Bell Street to the south. The site also incorporates a section of Smithfields, a pedestrianised highway which links to Market Street to the east.
2. There is a difference/change in levels across the site and this is evidenced via the access from Bath Road (ring road) and the car parking entrance made of New Street where it is clear that there is a complete floor difference.

3. This site currently accommodates some 8267m<sup>2</sup> of floor space excluding car parking, (5285m<sup>2</sup> being retail). This includes the vacant former Morrisons (Safeway/Presto) supermarket, the former Market Hall, individual shop units and Council offices. The Crown Centre is presently serviced from Crown Lane via an underground facility, with parking access via Bell Street and multi-storey car park off New Street. Pedestrian access is achieved from High Street, New Street, Smithfield's and via lifts and stairs from the car park. The scale of the existing structures of the centre and car park is of between three and seven storeys in height. The present car park is still presently utilised for parking, mainly serving the neighbouring municipal pool and whilst the undercroft is currently vacant but historically has been used as a skateboard centre and for tyre repairs.
4. The context of the site directly relates to the heart of the town centre of Stourbridge and is adjacent to Stourbridge Town Hall and library and the Grade II listed former Market Hall building. The main pedestrian entrance is of particular note, where the building has a strong visual presence from the High Street. The Crown Centre access from this point is within a designated the Stourbridge Town (High Street) conservation area, which accommodates a number of individually listed buildings and structures including the town clock.

## PROPOSAL

- 5 This planning application is for the demolition of the existing Crown Centre, including the former market hall and Morrison supermarket. The application also proposes the demolition of the existing Bell Street multi-storey car park.
- 6 To replace the demolished buildings a new mixed use development is proposed. This incorporates a 9336m<sup>2</sup> (gross) TESCO store (A1), 1467m<sup>2</sup> (gross) food court/mall area, 279m<sup>2</sup> (gross) of smaller retail units (A1, A2, A3, A4 and/or A5) and 557m<sup>2</sup> of office (B1) accommodation. Also proposed is a small extension to some of the retained units (included within the above floor space), together with a new entrance to the library and the Town Hall. Servicing to the proposed TESCO store would be at grade and will be accessed from Bell Street with high boundary walls to the Ring Road and Bell Street. Two levels of underground parking are proposed

below the site providing 543 spaces. Access to the car park would be from Bell Street and would exit into Crown Lane.

- 7 Also included with the application is the deletion of New Street, which would be replaced by Bell Street becoming two-way with a new signal controlled junction with the Ring Road. Also proposed is a new surface signalised crossing which would be located close to Envile Street. The pedestrian crossing would also be linked with a new signalised exit from Crown Lane.
- 8 Other works associated with the development are an enhanced town square between lower and upper High Street, a widened footway to Crown Lane, incorporating a cycle way. Works are also proposed to the footway to Bell Street including a modified access to the adjoining Stourbridge Institute, and a new crossing.
- 9 The proposed service yard a wall is proposed to both the Ring Road and to Bell Street, incorporates public art and planting.
- 10 The proposed TESCO store would be on two levels with the upper mezzanine level accessed by a travelator and lifts. The proposed store incorporates a café, storage space and staff accommodation, which includes a staff catering facilities.
- 11 The building will be contemporary design, although would in the main be faced in brick, stone and glass reflecting existing materials found within the town. The only elevation that will not be finished in these materials will be elevation to the service area which would be faced in metal cladding. The building has been to a certain extent been provided with active frontages with shops onto the new square, which also turns the corner into Crown Lane. Crown Lane would also where the access to the offices above the mall shops would be located. This again would provide some activity to the frontage.

- 12 Pedestrian access to the site would be from Market Street and from High Street in the same locations as at present. A travelator, stairs and lifts would provide pedestrian access from the underground car parks to the mall and main store.
- 13 The car parking would be designed principally with passive ventilation in mind, with wind catchers to the main roof to provide natural ventilation to the store. The construction includes roof lights to the main store and the mall to enable natural light ingress.
- 14 In addition to the above a temporary access is to be provided to the library and Town Hall whilst the works are carried out. This would be from Smithfield and includes a P09/1032/C1 ramp to allow access for the disabled, the less ambient and persons with children.
- 15 The application is accompanied by a Design and Access Statement, a Noise Assessment, a Air Quality Assessment, a Transport Assessment, a Ground Condition Report, a Flood Risk Assessment, a Bat Survey, a Planning and Regeneration Statement, incorporating a Retail and Heritage Impact assessments, a Statement of Community Involvement together with a Summary Statement. The applicant has also submitted coloured a three dimensional concept drawings of the site and surroundings showing the proposed building.

## HISTORY

APPLICATION No.	PROPOSAL	DECISION	DATE
81/50001	Erection Of Retail Shops Public Library Offices	Granted	29/01/81
81/52577	Erection Of Retail Shop Public Library Offices Market Hall And Associated Car Parking And Basement	Granted	11/01/82
P07/1599	Redevelopment of the Crown Centre to include food store,	Granted	23.4.08



	market hall, retail units and 75 residential units		
P08/1594	Re-development of the Crown Centre to include food retail store (A1), two retail units, new market hall and hotel (C1) with associated parking servicing and access (resubmission of approved application P07/1599)	Granted	13/01/2009
P10/1430	Listed Building Consent for partial demolition and external alterations of rear extension to Town Hall. Internal and external alterations associate	Current	Current
P10/1431	Conservation Area Consent for demolition of part of unlisted Crown Centre building	Current	Current

- 16 Planning applications P07/1599 and P08/1594 involved the retention of the existing Crown Centre with extensive alterations and extensions to form an enlarged supermarket and shop units, with an additional deck of car parking above the existing roof top car park.
- 17 Both applications sought to redevelop the existing multi-storey car park with shop units and a market hall on the ground floor. P07/1599 sought the provision of block of flats on top of the new multi-storey car park, and P08/1594, a hotel.
- 18 The height of both applications, particular due to the introduction of decked parking above the existing centre, together with accommodation above the replacement multi-storey car park meant the buildings were significantly higher than the existing Crown Centre and multi-storey car park.
- 19 The road layout around the perimeter of the site was unchanged by both proposals.

- 20 In parallel with this application is a parallel conservation area and listed building consent applications for the demolition of the existing Crown Centre.
- 21 Due to the works required the applicant is also seeking permission to enter in a Section 247 agreement, to modify/divert/extinguish highways, in or around the site.

## PUBLIC CONSULTATION

- 22 Consultation carried out neighbours adjoining the, together with the posting of a site notice, and the publication of an advert within a newspaper circulating within the area. Additionally the applicant has placed an advert within a local newspaper and held a public exhibition in Stourbridge library during the summer.
- 23 6 letters of objection (including 2 from same source) received, main issues raised:-
- Plans do not show enough detail in respect of traffic movements and flow
  - Concern about possible car parking charges and the time limits which may be imposed
  - Ring Road should become a *red route*
  - Disruption caused by works and potential loss of income
  - TESCO have a number of branches within the area already
  - Proposed store is too large
  - New store should be limited to food (convenience) retail only
  - Proposal store will affect smaller shops within the town
  - Overdevelopment of site, poorly relates to town, scale mass, bulk, Lack of human scale, out of context
  - Design and materials are out of keeping
  - Site was only developed 25 years ago and has become a *white elephant*
  - Lack of active frontages, particularly to ring road,
  - Changes to road layout will lead to congestion,
  - Transport assessment makes no reference to congestion caused by new signals on High Street, Amblecote
  - Potential conflict on traffic flow when lorries leave service yard

- First impression of site will be the service yard, and the screen wall proposal is a poor attempt at urban design
- Transport assessment underestimates additional traffic movements to store
- Car parking would be *privatised*
- Longer pedestrian distances to leisure centre etc
- Topography of site could have been worked into the design
- Levels changes between Crown Lane and ring road are significant, which be difficult for the elderly
- Existing subway is poor
- Will increase 15% increase in town centre floor space
- Store is eight times greater than the existing Waitrose store
- Retail mix will be reduced

## OTHER CONSULTATION

- 24 Group Engineer (Development): No objection subject to conditions. Due to the fallback there will be no need for contributions towards transport infrastructure
- 25 Head of Environmental Health and Trading Standards: Conditions to address concerns about noise disturbance to local residents from the night time use of the service yard & associated equipment, contaminated land issues & additional details for the proposed CHP unit are required”
- 26 Environment Agency: No objection subject to conditions.
- 27 Severn Trent Water: No objection
- 28 English Heritage: Happy for Council to consider application in accordance with national and local policy.
- 29 West Midlands Police: The design, layout and materials used for the public space in front of the development should be carefully considered. Car parks suffer from crime and anti social behaviour, best practice would be to secure entrances and

exits to car parks when the complex is closed, and restrict parking to certain areas during quiet periods. Encourage car park to meet *Park Mark* standards. Bollards to prevent vehicle penetration should be fit for purpose.

30 West Midlands Fire Service: No objection

31 EON (Central Networks): No objection

32 Head of Urban Design: Awaited at the date of agenda preparation.

33 Access in Dudley: The Council's Access Officer, and the applicant's architects have had meetings with this organisation, and where possible have taken into account their views, with modifications to the design and layout of the proposed development, including retention of the subway.

## RELEVANT PLANNING POLICY

- National Planning Guidance
  - PPS1 - Delivering Sustainable Development
  - PPS4 – Planning for Sustainable Economic Growth
  - PPS5 – Planning for the Historic Environment
  - PPG13 – Transport
- Black Country Joint Core Strategy
  - CSP2 Development Outside the Growth Network
  - CSP3 Environmental Infrastructure
  - CSP4 Place Making
  - CSP5 Transport Strategy
  - DEL1 Infrastructure Provision
  - EMP1 Providing for Economic Growth
  - CEN1 The Importance of the Black Country Centres for the Regeneration Strategy
  - CEN2 Hierarchy of Centres
  - CEN4 Regeneration of Town Centres

CEN5 District Centres and Local Centres

CEN8 Car Parking in Centres

TRAN2 Managing Transport Impacts of New Development

TRAN4 Creating Coherent Networks for Cycle and for Walking

TRAN5 Influencing the Demand for Travel and Travel Choices

ENV 1 Nature Conservation

ENV 2 Historic Character and Local Distinctiveness

ENV 3 Design Quality

ENV 7 Renewable Energy

ENV 8 Air Quality

- Unitary Development Plan (saved policies)

DD1 Urban Design

DD2 Mixed Use

DD3 Design of Retail Development

DD4 Development in Residential Areas

DD9 Public Art

DD10 Nature Conservation and Development

CR10 Conditions on Major Retail Development

STC1 Thoroughfares and Public Spaces

STC2 Street blocks

STC2(v) Block 5 - Crown Centre/Bell Street

AM13 Public Rights of Way

NC1 Biodiversity

NC6 Wildlife Species

HE4 Conservation Areas

HE5 Buildings of Local Historic Importance

HE6 Listed Buildings

HE8 Archaeology and Information

HE11 Archaeology and Preservation

EP7 Noise Pollution

- Supplementary Planning Guidance/Documents
  - Nature Conservation Supplementary Planning Document
  - Historic Environment Supplementary Planning Document
  - Parking Standards and Travel Plans Supplementary Planning Document
  - Planning Obligations Supplementary Planning Document
  - Design for Community Safety Supplementary Planning Guidance
  - PGN1. Office development
  - PGN4. Retail development
  - PGN5. Retail frontages
  - PGN6. Highway considerations in development
  - PGN7. The development of derelict, contaminated and unstable land
  - PGN 8. Archaeology
  - PGN 9. Replacement dwellings in the green belt
  - PGN 20. Access for people with disabilities
  - PGN 27. Shop front design and security
  - PGN 28. Hot food takeaway shops, restaurants and cafes (class A3 uses)

## ASSESSMENT

- 34 The main issues are
- Principle/policy
  - Design – Historic Environment
  - Design - General
  - Design – Public Realm
  - Regeneration
  - Neighbour Amenity
  - Accessibility
  - Highways Matters – General
  - Highways Matters – Servicing
  - 
  - Highways Matters – Parking
  - Highways Matters – Other
  - Ground Conditions

- Air Quality
- Climate Change
- Flooding
- Nature Conservation
- Public Art
- Access to Town Hall and Libraries
- Secure by Design Issues
- Planning Obligations
- Other Matters

### Principle/Policy

#### 35 **Unitary Development Plan**

The Stourbridge Town Centre Inset of the adopted Unitary Development Plan (UDP) places the application site within Block 5 and the Retail Core (Primary Shopping Area) of the town. Policy STC2 (v) seeks to accommodate retail use. The Bell Street multi-storey car park forms part of Opportunity Site B of the inset for redevelopment potential. The policy notes opportunities for redevelopment to present a more active and attractive frontage to the ring road, traffic management in Bell Street, replacement of the Multi-storey car park (and link to the Crown Centre) and reinstatement of the former route of New Street for pedestrian access. Preferred uses for Block 5, which includes the opportunity site are retail, and within the retail core, food and drink uses are also preferred. Within the block, the use of upper floors for office use is desirable.

#### 36 **Black Country Joint Core Strategy**

The Black Country Joint Core Strategy emerging Policy CEN4 – Regeneration of Town Centres, seeks to encourage new convenience development in Town Centres and supports proposals to extend or refurbish existing stores where they are well integrated and to serve to anchor the centre as a whole.

37 **Government Policy**

Planning Policy State 4 – Planning for Sustainable Economic Growth (PPS4) provides national planning policy guidance on town centres, retailing and economic development. PPS4 reinforces the town centre first approach for retail development. There is no doubt that the site in question is the most sequentially preferable site for this retail development. Furthermore as stated in the Planning and Regeneration Statement, the principle of this type of development has already been established through the previous 2007 and 2008 applications which in effect are ‘fall back’ schemes. The net convenience of the Tesco scheme is similar to that 2008 Modus scheme, with the net comparison floor space increasing by 525m<sup>2</sup>

38 PPS4 requires the all planning applications for economic development to be assessed against the five impact tests outlined in Policy EC10.2, which also states that Local Authorities should adopt a positive and constructive approach towards planning applications for economic growth. The applicant has assessed the impact of their proposal against the criteria set out in paragraph EC10.2;

- climate change
- accessibility of the proposal
- high quality and inclusive design
- impact on economic and physical regeneration in the area
- the impact on local employment

39 The matters stated above are given more consideration elsewhere in this report.

40 Given the proposal site is an in-centre development in accordance with an up-to-date development plan, PPS4 Policy EC15 ‘A Sequential Assessment’ is not required.

41 In accordance with Policy EC16, the applicant has examined the impact of the proposed food store on Stourbridge Town Centre, considering that the food store will have a positive impact of around £5.1m on the existing Stourbridge Town Centre shops, thus enhancing its vitality and viability.



42 In considering the appropriateness of the scale, key points identified include the;

- Scale of the proposed food store is necessary to actively compete with existing main food stores and claw back shoppers and trade to Stourbridge which will significantly enhance its vitality and viability
- Scale of the proposed food store is required to provide the critical mass necessary to provide an anchor function and act as a catalyst for wider Town Centre regeneration.
- Proposed food store is required to enhance the range and quality of goods available in the Town Centre to meet the qualitative need for a large food store.

43 Furthermore, the scale is considered appropriate, as the adopted UDP identifies a need for a major food store in Stourbridge Town Centre.

44 **Policy/Principle Conclusion**

There are no Planning Policy objections to the comprehensive redevelopment of the site, as it would provide improved retail offer within the defined retail core, accompanied by other appropriate town centre uses. The proposals accord with national policy guidance set out in PPS4 and at the local level, Policy STC2 (V) 'Block 5: Crown Centre/Bell Street' in the Dudley UDP.

Design - Historic Environment

45 As regards the historic environment the Tesco proposal has the potential to impact upon the Stourbridge High Street Conservation Area itself, within which the north eastern part of the Crown Centre currently falls, and upon its' setting and also the setting of a number of statutorily and locally listed buildings within the vicinity of the proposed development.

46 In considering such proposals the Council has a statutory duty to pay special regard to the desirability of preserving or enhancing the character or appearance of the

conservation area and, equally, to have special regard to potential impacts upon statutorily listed buildings and their settings. Unitary Development Plan Policy (HE5) commits the Council to resist proposals that would have an adverse impact upon the setting or context of Locally Listed Buildings.

- 47 In all of these respects design discussions with the applicants have resulted in a proposed scheme that is considered to be acceptable taking into account the physical, topographic and operational constraints presented by the urban form in this tight town centre location.
- 48 Of particular sensitivity is the frontage onto High Street at the confluence with Coventry Street, Market Street and Lower High Street (“Crown Square”) and to a slightly lesser extent that onto Crown Lane. The proposed new store building is of an overall height and scale (three storeys) that generally accords with predominant building heights within the conservation area. In this respect it relates particularly well with the adjacent grade II listed Market Hall (4-6 Market Street). The new build here also stands off from the listed building at a respectful distance, with the new entrance area to the shopping centre linking the two aforementioned structures being considerably recessed. Taller parts of the building beyond this are accommodated by falling ground levels that have the effect of reducing visual impacts at these locations so that the proposal as a whole is not unduly dominant.
- 49 The architectural rhythm of the building in this sensitive area is expressed as a series of evenly spaced vertical components that generally mirror the subdivisions between buildings within surrounding historic streets. The proposed use of materials, with predominantly stone finish panels of walling also accords well with the historic use of such materials on adjacent buildings and more generally within the conservation area. Breaking up the stone panels with glazing that includes shop fronts wherever possible provides active frontages to add to the areas general vibrancy.
- 50 Taking all of the above into account it can be concluded that the proposal does not adversely impact upon the general character and appearance of the Stourbridge

High Street Conservation Area nor does it adversely affect the setting of the areas statutorily and locally listed heritage assets. Indeed it can be considered that when set against the current status quo as represented by the Crown Centre the scheme constitutes a welcome overall enhancement.

### Design - General

- 51 The existing centre was constructed in the 1980s and is to be considered to be of its time. Whilst the building through the use of different roof-scapes, particularly to the Crown Lane elevation, provides some interest and articulation, the building in the main is considered to be comparatively bulky with heavy massing, which is compounded by the lack of any activity to either the Crown Lane or ring road elevations, even at the upper levels.
- 52 The building also suffers from a bright red engineering brick which is untypical of the more subtle brick colours in the rest of the town. The building due to its bulk and aggressive angular design, particularly to the town square, means that it unnecessarily competes with the adjoining town hall which should be the focal point of the square. The building, also relates poorly to the ring road, again due to the lack of activity to this elevation.
- 53 The adjoining multi storey car park which was built in the late 1960s/early 1970s is again, a building of its time. The building is considered to be fairly monolithic in its scale, which is further exacerbated by its use of materials and excessive horizontal emphases, and as such dominates the western edge of the town centre.
- 54 The previously approved schemes retained the majority of the existing Crown Centre, although a further level of car parking was proposed above the existing roof top parking, thus increasing its scale and massing. The Bell Street Car Park which was to be redeveloped, was to have its height increased, particularly as housing and hotel was to be located on the upper level of the building, which would have taken its height well beyond the existing car park. The photographs submitted with the Design and Access Statement clearly illustrate this.

- 55 The current proposal attempts to overcome the deficiencies of the existing centre, and the design compromise that were required to ensure the two previous schemes worked operationally, and this is part helped by clearing the existing centre, and car park, and incorporating New Street into the site boundary.
- 56 The application site, is severely constrained by its town centre site, with the location of listed buildings, levels and the need to address a number of frontages being necessary. It is considered that this elevation has succeeded in achieving these aims, in that the proposed elevation would be less assertive than the heavy brick of the existing with a much greater use of glass, although the use of stone provides a link with materials found elsewhere in the town centre. The set back of the building also assists in this purpose and allows improved views of the town hall. The elevation also succeeds in providing an active frontage with shop fronts onto the square, which could have the potential allow shops or cafes to spill out onto the square. The elevation successfully turns the corner onto Crown Lane, and through the use of split level units manages to ensure active frontage also follows through.
- 57 The Crown Lane elevation has been broken down into a number components to reduce the mass and bulk of the building to this elevation. For example this involves projections to the main building and use of different materials. An active frontage has been provided in part by the access to the office accommodation and underground car park, together with high level windows to the main store. Materials to this elevation include brick, stone and glass. The height of this elevation is similar to the existing building, and less than that previously approved.
- 58 Consideration has also been given to the elevation to the ring road. This is difficult elevation to try and address from a design point of view due to the change in levels, the need to ventilate the car park and the size of the elevation. The design, like the elevation to Crown Lane, has been principally designed to break the building down into smaller elements, in an attempt to reduce the mass of the building. The elevation has been designed to provide active frontage at a higher level, with views into the store, the café area and the staff area. This would be an improvement over

the existing and approved schemes where this elevation turns its back on the ring road.

- 59 The use of windows at a lower level were looked at, however, it was not possible due the siting of plant and store rooms , which could not be easily accommodated elsewhere without either affecting the serving arrangements or the high level active windows to Crown lane. Materials would reflect those used elsewhere on the building, such as brick, stone and glass.
- 60 A significant design issue has been the need to screen the service yard, but also ensure the screen wall adds some interest to the ring road, elevation as well as “announcing” the access to the store. The wall has been adapted to provide a screen for public art.
- 61 Overall, the design of the building is considered to be an improvement to the existing centre, and has a number benefits over the two previously approved schemes, particular in terms of height and integration with the wider area, including the use of more appropriate materials.

#### Design – Public Realm

- 62 A scheme for the town square has been worked up with the applicants, which would enable the area to be refurbished with new paving and landscaping. The square has been designed as a meansd of linking the redeveloped Crown Centre with the wider town centre. The delivery and detailed design of the square will be controlled by planning condition.

#### Regeneration and Employment

- 63 The site currently consists of the Crown Centre, with car park above and the adjacent Bell Street multi-storey car park. The former was constricted in the mid 1980s and was originally anchored by a market hall, supermarket and unit shops.

The scheme also incorporated offices, registry office and new library and entrance to the town hall.

- 64 The centre is currently mostly empty with the supermarket, last operated by WM Morrison closing as has the market hall. All that remains are a couple of small shop units and the library. Due to the centres mostly empty nature it could be perceived as being unsafe by many potential users. This effect is not helped by low ceiling heights of the existing centre, the lack of natural surveillance (particularly at night), the lack of natural light apart from the central square. Moreover, the open nature of building, with the lack of doors to the malls and the open central square can make the centre cold, windswept and uninviting during inclement weather.
- 65 The Bell Street multi storey car park is understood to be suffering from structural problems with part of the building being out of bounds, with also its appearance and circulation space becoming tired and dated.
- 66 Therefore, the redevelopment of these two buildings offers an opportunity to transform the site with a large modern supermarket, which Stourbridge presently does not have. It also provides replacement car parking with convenient access to the proposed store and wider town centre, together with improvements to public realm and rationalisation of access to the town centre.
- 67 From an employment point of view, the applicant implies the proposed development would create between 400 and 465 jobs. In addition the proposal also provides opportunities for smaller business with a number of smaller units, together with an element of new office space.

#### Neighbour Amenity

- 68 The nearest residential units to the site are located on the western side of the Ring Road, in Hepland Road and Baylie Court.

- 69 In terms of light and overlooking the proposed development would not have significant harm in that there are already tall buildings, notably the existing Crown Centre and the Bell Street multi-storey car park on the site.
- 70 However, a new potential noise source is to be created, the service yard to the proposed superstore located behind the screen wall to the Ring Road and New Street, and as such the Head of Environmental Health and Trading Standards have been consulted.
- 71 During the day time the main back ground noise source is considered be traffic noise from the ring road. However, overnight when traffic is lighter, the potential noise impact of the service yard would become more apparent. Therefore, the Head of Environmental Health and Trading Standards wishes to impose conditions on the operation of the service yard, including delivery hours, use of the home delivery vehicles and the waste compactor.
- 72 The main plant to the site (i.e. chiller units, air conditioning etc) would be located within a plant well. The use of such a well reduces the potential for noise to be carried towards nearby housing, and as such the Head of Environmental Health and Trading Standards is satisfied with this, subject to a noise condition being imposed.
- 73 The Head of Environmental Health and Trading Standards is also satisfied that the store operation would cause any harm to amenity and therefore no opening hours condition is proposed.
- 74 Some concerns have been raised by an adjoining commercial premises over disturbance which may be caused by the proposed building works. Unfortunately this is not a matter that normally be considered as of part of the planning process.

#### Accessibility

- 75 The site is highly accessible site being located on the Stourbridge ring road which forms part of the Borough Strategic highway network. The location of the store has

the advantage that car-borne journeys can be catered for without the need for traffic having to enter the town centre. This includes the servicing for the main store as well.

- 76 The site is also located close bus stops on the opposite side of the ring road. Buses from these stops include services running to many parts of Stourbridge and the wider Borough and beyond. Also within walking distance of the site is the town's main bus station (presently being refurbished) and the Stourbridge Town railway station which provides a shuttle service to the main line station at Stourbridge Junction which is on the busy Birmingham Snow Hill to Kidderminster/Worcester line.
- 77 The site is also accessible by foot and cycle, with the applicant proposing improvements to both, with a new surface crossing over across the ring road and a new cycle lane to Crown Lane, which would allow people to cycle to the store without having to use the ring road, which would need to do at present due to the existing one way system within the town centre. The applicant has submitted sufficient details in relation to these works to satisfy the Highway Authority that the proposals are ready to move on to a detailed design stage with a satisfactory resolution, dealt with outside of the planning process.
- 78 In terms of pedestrian access the provision of a new surface crossing would allow a more direct route from people travelling along Enville Street reinstating a direct link which has not been possible since the construction of the ring road in the early 1970s.
- 79 It was initially hoped that new crossing would replace the existing subway under the ring road, which is considered to be an unwelcoming entrance way into the town centre. The removal would have also overcome perceived secure by design issues associated with such crossings as well as complying with the aspiration of the draft Stourbridge Area Action Plan which seeks to improve pedestrian links across the ring road. However, it is not possible for the new surface crossing to comply with disability discrimination regulations, as the gradient up Crown Lane is fairly severe



and therefore the subway will need to be retained unless another solution can be found, although it is understood that the subway presently does not comply with disability discrimination regulations on its western side.

80 The retention of the subway may lead to Secure by Design concerns. However, minor amendments to the alignment of the highway, provision of CCTV camera to overlook this area and other features to improve visibility should overcome these issues. A Grampian condition to ensure the satisfactory resolution of this point would be acceptable.

81 Generally the Local Authority would enter into a Walkways Agreement for the revised Crown Centre concourse, which would establish the hours for public access particularly when the store is closed and invite the police to control the concourse as they would any other public space. However, it would appear that the Local Authority as land owner can't enter into an agreement with itself (Sec. 35 Highways Act 1980) and this issue will need to be resolved by planning condition.

#### Highway Matters - General

82 The development proposes a number of major highway improvements surrounding the site:

#### 83 **Bell Street & Bath Road**

The scheme proposes to stop up New Street, through a Sec. 247 Order, incorporate this land into the development site and allow vehicles to exit the town centre via a new traffic signal controlled junction at Bell Street/Bath Road.

84 The provision of these traffic signals will also necessitate the repositioning of the pedestrian crossing to the south of the junction along with the existing gantry for the signal heads, amendments to the existing coach drop-off on Bath Road and the re-grading of the carriageway and footways on Bell Street.

85 The applicant has submitted sufficient details in relation to these works to satisfy the Highway Authority that the proposals are ready to move on to a detailed design stage with a satisfactory resolution, dealt with outside of the planning process.

86 **Crown Lane & Bath Road**

It is proposed to provide an at-grade pedestrian crossing facility on Bath Road in-association with the signalling of Crown Lane as a fourth arm of the existing signals at Bath Road/Enville Street. The signalling of Crown Lane is required to deal with the additional demand created by having the egress from the developments car park located on Crown Lane.

87 **Conclusion**

The applicant in close consultation with the Highway Authority has modelled the new and amended highway layout to assess its impact on highway capacity in the vicinity of the development. The Highway Authority is now satisfied that the proposed highway works will not have a detrimental affect on the operational capacity of Bath Road.

Highway Matters – Servicing

88 Parking Standards and Travel Plans Supplementary Planning Document has an A1 Retail operational parking standard of a minimum loading and unloading space of 50 sq.m for units with a Gfa from 500 sq.m to 1000 sq.m, 100 sq.m for units with a Gfa from 1000 sq.m to 2000 sq.m and 150 sq.m for units with a Gfa of more than 2000 sq.m. Service areas for loading and unloading must be laid out to allow lorries to enter and exit the site in forward gear.

89 The proposed food retail store has a dedicated service which is served directly from the car park access, whilst the other units within the Crown Centre including the Town Hall and Library have access to the service bay on the intermediate car parking level.

90 The service area on the intermediate car parking level won't be able to cater for pantechicon sized vehicles or articulated vehicles. However, all of the units within the Crown Centre, with the exception of the Town Hall and Library will be in the control of Tesco and they will be able to exert significant control over these units to ensure that they are only serviced by vehicles capable of using the loading bay. With the exception of the very largest vehicles servicing the Town Hall this facility should also cater for the Town Hall & Library.

#### Highway Matters - Parking

91 The developments car parking provision is split over two levels. The intermediate floor (level -1) provides 230 general spaces including 2 spaces with electric charging facilities, 14 mobility impaired parking spaces, 13 parent & child spaces and 4 spaces set aside for employees of the Town Hall. This level also provides a service area for the other units within the Crown Centre, including the Town Hall & Library. The basement floor (level -2) provides 245 general spaces including 2 spaces with electric charging facilities, 24 mobility impaired parking spaces, 13 parent & child spaces.

92 The development provides 543 parking spaces in total, including 38 mobility impaired parking spaces. This amount of parking provision doesn't exceed the maximum parking requirement, as calculated using the Parking Standards and Travel Plans SPD.

93 Parking Standards and Travel Plans Supplementary Planning Document has a minimum Mobility Impaired parking standard for parking areas where the public have access of 4 wider parking spaces for car park over 40 spaces and then 1 wider space for every 15 standard parking spaces thereafter. Therefore the retail development car park should provide:  $230 + 245 = 475$  parking spaces – 40 = 435 spaces/15 = 29 + 4 = **33 mobility impaired spaces**.

94 As specified in PPG 13, parking for disabled people will be additional to the general level of parking permitted under the maximum parking standards.

Each reserved space should be at least 4.8 metres (m) by 2.4m, with a 1.2 metre wide hatched access zone located on one side and rear of the parking space facing the vehicular route. The overall size of the disabled parking including the access zone will be 6m by 3.6m.

- 95 The management of the car park (i.e. pricing and length of stay) has been raised as part of the consultation process. However, the management of the parking is not a matter which can normally be controlled through the planning process, and is normally matter for the car park operator. The only concern of the planning authority, and the Group Engineer (Development) is that is sufficient parking space within the proposed development, and that is available for the users of the development. In the case of the former the Group Engineer (Development) is satisfied. The second of these is effectively self enforcing, due to the need to attract visitors to the store.

#### Highway Matters – Other

- 96 Generally, the Highway Authority would not support doors opening out over the public highway. There are a couple of situations where this occurs on this scheme, at the means of escape (MoE) onto Crown Lane and the combined MoE/pedestrian access onto the footway serving the subway. In view of the expected frequency of use for the MoE on Crown Lane, the doors opening out over the highway wouldn't be an issue. However, the combined pedestrian access/MoE onto the footway linking the subway will have a much greater frequency of use, which would be an issue in-relation to pedestrian safety and may create secure by design concerns with the doors obstructing visibility on the footway. Revisions to the layout of this MoE/pedestrian access will be required, but can be controlled by condition.
- 97 The proposed scheme will also involve the provision of new Traffic Regulation Orders (TRO's) and amendments to existing TRO's in the vicinity of the development:

- a. The loss of the limited waiting parking on the southern side of Crown Lane.
- b. Amendments to the bus stop on the southern side of Crown Lane.
- c. The revisions to Crown Square will involve the whole scale revamping of the TRO's covering this area.
- d. The introduction of the pedestrian crossing on Bell Street will require a TRO.
- e. The provision of two No. on-street disabled parking bays on Market Street. Locations to be agreed.

98 The developer will be required to make a contribution of £5000 to the Local Authority for the advancement of each of the Traffic Regulation Orders discussed above. The contribution for the TRO should be received by the Local Authority prior to development commencing on site.

99 The scheme also requires amendments to the existing vehicular access to the Social Club car park from Bell Street. The Highway Authority would require that this amended access be via a footway crossing to improve pedestrian priority in the area.

#### Ground Conditions

100 Due to the nature of the Black Country there is potential for contamination to be present on the site. However, the Head of Environmental Health and Trading Standards in this case is happy to impose conditions in respect of this, particularly as the previous applications were submitted with supporting information.

101 The Environment Agency initially objected as the application site is within a controlled waters area. However, the applicant submitted additional information and the Environment Agency is now happy for this matter to be conditioned.

### Air Quality

- 102 Whilst the Borough is designated an Air Quality Management Area, due to the relative free flow of the ring road, there are no significant issues within the area. It is therefore considered the proposal will have no detrimental impact. Some of the measures, referred to in the climate change section of this report would contribute towards air quality improvements.

### Climate Change

- 103 As stated above, PPS4 requires consideration to be given to climate change when assessing applications of this nature. In this case the applicant has provided a sustainability statement with the application.
- 104 Measures the applicant has included to improve the sustainability of the development include electricity charging points within the car park, passive ventilation to the shop and car park, roof lights to the main store as well as the provision of a combined heat and power plant (CHP).

### Flooding

- 105 Due to the scale of the application, there is a requirement in accordance with PPS25 to provide a Flood Risk Assessment with the application. Whilst the site is outside of the Stour valley flood plain, the impact of surface run off from the site has to be considered.
- 106 Initially some concerns were raised by the Environment Agency, but the provision of further information has allayed these concerns, and they are now prepared to support the application on flood risk grounds, subject to the imposition of appropriate conditions.

### Nature Conservation

- 107 The application has been accompanied by a bat survey of the application site. The survey concludes that the majority of the buildings have no potential as bat roosts. The only part which may be suitable, the underground service area, still has negligible to low potential.
- 108 Surveys, undertaken only indicated one bat entering the service area, with no commuting activity being recorded. The potential for foraging locality is extremely limited. The report, therefore recommends that precautionary measures are taken when demolishing the building. An informative is proposed which remind the applicant of the surveys recommendations.

### Public Art

- 109 The applicant is proposing the provision of new public art to the screen wall to service yard to the ring road and New Street. The concept which is a “living wall” is to provide an entrance feature to the town centre. In addition the applicant this seeking to relocate the automaton, and the town coat of arms mosaic. The provision of these items is to be controlled through planning conditions.

### Access to Town Hall and Library

- 110 The Crown Centre at present provides the main access to the above well used town centre facilities. To ensure these facilities continue to be available whilst the works are carried out a temporary access will be provided from Smithfield. This temporary access will incorporate a ramp to allow an access for all potential users. Once the works are complete the existing access arrangements will be reinstated.
- 111 Servicing for the library and/or town hall would be from the car park for lighter vehicles, with serving for larger vehicles from Market Street as of present. The Group Engineer (Development) is satisfied with this.

### Secure by Design Issues

- 112 The existing centre was constructed during the 1980s, and the adjoining car park in the 1960s, and have a number of design deficiencies from a secure by design point of view. For example the quality of lighting is poor in places, with stairways having little natural surveillance.
- 113 In addition the existing Crown Centre, due the number of empty units ensures a relatively low foot fall which increases the fear of crime, which is increased further at night, and when the library and town hall and closed. The provision of a main store which would be open in the evening, would significantly improve this situation.
- 114 West Midlands Police have commented on the application, and are generally satisfied with the proposal. Their main concern mostly relate to the car park management, and the preventing egress of motor vehicles into the centre.
- 115 The retention of the subway, continues to pose secure by design issues, but this is unavoidable without causing difficulties for people unable to climb the steep gradient up Crown Lane to the new ring road crossing. However, as stated above, the provision of CCTV should overcome this concern.

### Planning Obligations

- 116 Ordinarily, a development of this scale would require the contributions towards highway infrastructure improvements, public realm, public art, nature conservation and an employment and training commitment
- 117 The first of the previous applications was submitted prior to the adoption of the Planning Obligations Supplementary Planning Document and the scope for planning gain was more limited than would be required now. Similarly the second application which was similar, made use of the earlier fall back position. Coupled with is position the schemes under consideration were a reworking and extension of the existing centre. Despite this new car parking, and new square, and relocation of



public art was proposed by the developer. However, this fall back position still is a significant factor in considering this application, and the scale of the obligations which can be negotiated. The Group Engineer (Development) shares this view in that the level of development that already has approval, there are no grounds for requesting a contribution towards Transportation Infrastructure.

118 However, despite this fall back the applicant has offered, or has agreed to provide the following infrastructure:-

- Replacement car park (under proposed store), which will be available for shoppers and other visitors to the town centre (almost 600 spaces)
- Reconfigured and simplified road network, including the provision of a new signal controlled junction with Bell Street
- New pedestrian crossing to Bell Street, to allow improved access to leisure centre
- Cycle lane and widened footway to Crown Lane
- Provision of new surface crossing over the ring road at Enville Street
- New public art feature to ring road/New Street
- Relocation of existing public art (automaton and mosaic)
- Public Realm improvements around the site, including the refurbishment of the “town square”
- New entrance to library/town hall
- Electric vehicle charging points (8 number)
- Employment and training commitment

119 The provision of the above infrastructure is considered to be relevant and is sufficient to off set the impact of the development particularly considering the fall back position, and is considered to be a significant improvement on the previous negotiated planning obligations for the site.

120 The provision of the transport infrastructure works would allow better access to the town centre, by car, foot and by cycle. In the case of the new surface crossing, this

contributes towards the aspirations of the draft Stourbridge Area Action Plan, where better pedestrian links across the ring road are encouraged and proposed, which would encourage more people to walk into the town centre.

- 121 As stated above, the provision of the cycle lane along Crown Lane, would significantly improve access for cyclists, which can presently only enter the town centre legally from the south and west by either circumnavigating the ring road, or by pushing the cycle along the footway.
- 122 The provision of the electric charging points is welcomed. This will allow the store to be accessed by electric vehicles which are likely to be more common in future. The provision of such facilities pre-empts the revised Supplementary Planning Document on Planning Obligations, where such facilities would be required in larger developments.
- 123 The public realm works, relate to the perimeter of the site, mostly to Crown Lane and to the town square. These works are way of integrating the new development into the existing townscape. The refurbishment of the town square would become a significant focal point for the town and would act and as transition between the new Crown Centre and the rest of the town centre, encouraging people into the rest of the town centre. The square is also designed that shop units within the new development can “spill out” out onto the square. The provision of the works is considered to be in accordance with the SPD.
- 124 However, The design of the Square remains an issue. The disparate needs of Access in Dudley are difficult to incorporate into a design which produces a truly shared space capable of accommodating public functions and still serves as vital vehicular link for properties in Coventry Street. The final design of the Square will therefore be dealt with by a planning condition.
- 125 The provision of the new public art to the ring road is welcomed and is accordance with the provisions of the Development Plan, and the SPD.

## Other Matters

- 126 Since the previous two applications were determined the Government advice on referrals to the Secretary of State on retail applications within town centres has been amended, and as such the Council no longer has to refer the application to the Government Office before the Council is allowed to grant planning permission.

## **CONCLUSION**

- 127 The proposed development is acceptable in principle due to its location within an existing town centre. The development is to be of acceptable design and integrates well with the town centre and the adjoining conservation area and listed buildings. The development would have no adverse impact on the highway network, and would have sufficient parking to meet the needs of the proposed development. In addition the development would improve the town centres accessibility and provide a significant regeneration opportunity. The scheme provides mitigation against its impact with improved public realm and a simplified highway network. The proposed development similarly has no adverse impact on nature conservation, air quality or residential amenity. Consideration has been given to policies DD1 Urban Design DD3 Design of Retail Development DD4 Development in Residential Areas DD9 Public Art DD10 Nature Conservation and Development CR10 Conditions on Major Retail Development STC1 Thoroughfares and Public Spaces STC2 Street blocks STC2(v) Block 5 - Crown Centre/Bell Street AM13 Public Rights of Way NC1 Biodiversity NC6 Wildlife Species HE4 Conservation Areas HE5 Buildings of Local Historic Importance HE6 Listed Buildings HE8 Archaeology and Information HE11 Archaeology and Preservation and EP7 Noise Pollution of the Dudley Unitary Development Plan and policies CSP2 Development Outside the Growth Network CSP3 Environmental Infrastructure CSP4 Place Making CSP5 Transport Strategy DEL1 Infrastructure Provision EMP1 Providing for Economic Growth CEN1 The Importance of the Black Country Centres for the Regeneration Strategy CEN2 Hierarchy of Centres CEN4 Regeneration of Town Centres CEN5 District Centres and Local Centres CEN8 Car Parking in Centres TRAN2 Managing Transport Impacts of New Development TRAN4 Creating Coherent Networks for Cycle and for Walking TRAN5 Influencing the Demand for Travel and Travel Choices ENV 1

Nature Conservation ENV 2 Historic Character and Local Distinctiveness ENV3  
Design Quality ENV 7 Renewable Energy and ENV 8 Air Quality  
of the Black Country Joint Core Strategy.

## RECOMMENDATION

- 1) It is recommended that the application be approved subject to the following conditions:
- 2) That the applicant be invited to make an application to the Secretary of State, under Sec. 247 Town & Country Planning Act 1990 to close, create and improve highways as shown on Drawing No. (TBC) to enable development authorised by planning permission to take place. The full costs and works to the Local Authorities satisfaction shall be met by the developer.

### Reason for approval

The proposed development is acceptable in principle due to its location within an existing town centre. The development is to be of acceptable design and integrates well with the town centre and the adjoining conservation area and listed buildings. The development would have no adverse impact on the highway network, and would have sufficient parking to meet the needs of the proposed development. In addition the development would improve the town centres accessibility and provide a significant regeneration opportunity. The scheme provides mitigation against its impact with improved public realm and a simplified highway network. The proposed development similarly has no adverse impact on nature conservation, air quality or residential amenity. Consideration has been given to policies The proposed development is acceptable in principle due to its location within an existing town centre. The development is to be of acceptable design and integrates well with the town centre and the adjoining conservation area and listed buildings. The development would have no adverse impact on the highway network, and would have sufficient parking to meet the needs of the proposed development. In addition the development would improve the town centres accessibility and provide a significant regeneration opportunity. The scheme provides mitigation against its impact with improved public realm and a simplified highway network. The proposed development

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The decision to grant planning permission has been taken with regard to the policies and proposals in the adopted Dudley UDP (2005) and to all other relevant material considerations.

The above is intended as a summary of the reasons for the grant of planning permission. For further detail on the decision please see the application report.

### Informatives

The applicant is drawn to the recommendations of the bats survey submitted with the application.

Buildings are frequently used as roosting and nesting sites by bats. Bats and their roost sites are protected under the 1981 Wildlife and Countryside Act. In the event of bats being present, Natural England must be contacted on 01453 764450 immediately for advice on the best way to proceed.

West Midlands Police request that the car park should meet *Park Mark* standards.

This notice does not provide any consent under the Town and Country Planning (Advertisement) Regulations.

The Environment Agency recommends that developers should:

- 1) Follow the risk management framework provided in CLR11, Model Procedures for the Management of Land Contamination, when dealing with land affected by contamination.
- 2) Refer to the Environment Agency Guiding Principles for Land Contamination Reports for the type of information that we require in order to assess risks to controlled waters from the site. The Local Authority can advise on risk to other receptors, e.g. human health.
- 3) Refer to our website at [www.environment-agency.gov.uk](http://www.environment-agency.gov.uk) for more information.

Conditions and/or reasons:

1. The development hereby permitted shall be begun before the expiration of three years from the date of this permission.
2. No development shall begin until an assessment of the risks posed by any contamination has been submitted to and approved by the Local Planning Authority. Such an assessment shall be carried out in accordance with authoritative UK guidance.
3. Where the approved risk assessment (required by condition 2) identifies contamination posing unacceptable risks, no development shall begin until a detailed scheme to protect the development from the effects of such contamination has been submitted to and approved by the local planning authority.
4. Unless otherwise agreed in writing with the LPA, the approved scheme (required by CL01b) shall be implemented and a verification report submitted to and approved by

the LPA, before the development (or relevant phase of the development) is first occupied/brought into use.

5. No development shall begin until an assessment of the risks posed by any ground gases or vapours has been submitted to and approved by the Local Planning Authority. Such an assessment shall be carried out in accordance with authoritative UK guidance.
6. Where the approved risk assessment (required by Condition 5) identifies ground gases or vapours posing unacceptable risks, no development shall begin until a detailed scheme to protect the development from the effects of such contamination has been submitted to and approved by the local planning authority.
7. Unless otherwise agreed in writing with the LPA, the approved scheme (required by Condition 6) shall be implemented and a verification report submitted to and approved by the LPA, before the development (or relevant phase of the development) is first occupied/brought into use.
8. No boiler shall be used to serve the development until details of the location, height, design and visual appearance of its chimney have been submitted to and approved in writing by the local authority.
9. No delivery vehicles whether loaded or unloaded shall enter or leave the site, nor shall delivery vehicles be unloaded, before the hours of 07.00 hrs nor after 23.00 hrs on any day.
10. The compactor shall not be operated between 07.00 hrs and 23.00 hrs on any day.
11. Dot.com (home delivery) activities shall take place only between the hours of 06.00 to 2300 hrs Monday to Saturday, and 0900 hrs to 1700 hrs on Sundays and Public Holidays.
12. The noise rating level of any fixed plant and/or machinery associated with the development shall not exceed background noise levels at any nearby dwelling by more than 5dB(A), as assessed under the methodology of BS 4142 (1997) (Method for rating industrial noise affecting mixed residential and industrial areas) and/or its subsequent amendments.
13. A scheme detailing the developer's commitment towards economic and community development through job opportunities using locally sourced labour and materials for the implementation of the development hereby approved shall be submitted to, and approved in writing by, the LPA prior to the commencement of works. Development shall be implemented in accordance with the approved details, unless otherwise agreed in writing by the LPA.
14. None of the development hereby approved shall be first commenced until precise details of the relocation of the automata (and its repiar) and the Borough of Stourbridge mosaic have been submitted to and approved in writing with the Local Planning Authority. The automata and mosaic shall thereafter be relocated in accordance with approved details and shall be installed prior to the main store first opening. The automata and the mosaic shall thereafter be retained and maintained for the life of the development.
15. None of the development hereby approved shall be first commenced until precise details of new on site public art works have been submitted to and approved in writing by the Local Planning Authority. The works shall thereafter be carried out in accordance with the approved details and shall be installed prior to the main store first opening. The art works shall thereafter retained and maintained for the life of the development.

16. None of the development hereby approved shall be first commenced until the location of eight electric vehicle charging points (4 on each level) within the car park have been submitted to and approved in writing by the Local Planning Authority. The charging point shall be provided in accordance with the approved details and shall be retained for the life of the development. The store shall not first trade until the charging points have been provided in accordance with the approved plans.
17. Development shall not begin until a surface water drainage scheme for the site, based on sustainable drainage principles and an assessment of the hydrological and hydro geological context of the development, has been submitted to and approved in writing by the local planning authority. The scheme shall subsequently be implemented in accordance with the approved details before the development is completed.

The scheme shall also include:

1. Limiting the rate of surface water run-off generated by the site to discharge at a rate of not more than the discharge rate from the site 1:100 20% to be no more than 240.96l/s ,as outlined in the updated FRA.
  2. Provision of attenuation storage volume on the total site to retain the 100-year 30% flow event volume assuming the discharge rate given above.
  3. Demonstration that the detailed surface water system proposed will also provide water quality improvements by implementing the Suds methods as outlined in the FRA.
  4. Details of how the entire surface water scheme shall be maintained and managed after completion.
  5. Detailed assessment of the performance of the surface water system for the 30-year and 100-year 20%flow cases.
18. The development hereby approved shall not be first commenced until detailed plans and specifications for the town square and other public realm improvements around the site have been submitted to and approved in writing by the Local Planning Authority. The town square and other public realm improvements shall thereafter be carried out in accordance with the approved details and the development shall not be first occupied, unless otherwise agreed in writing by the Local Planning Authority, until the approved town square and other associated public realm improvements have been carried out in accordance with the approved details.
  19. No part of the development hereby permitted shall be commenced until full details of soft landscaping works have been submitted to and approved in writing by the Local Planning Authority. These details must include, where appropriate, planting plans, written specifications, a schedule of plants including species, plant sizes and proposed numbers/densities and a programme of implementation. Plans must also include accurate plotting of all existing landscape features.

The works approved as part of this condition shall be completed in the first planting



season following first occupation of the development.

Any trees or shrubs planted in pursuance of this permission including any planting in replacement for it which is removed, uprooted, severely damaged, destroyed or dies within a period of five years from the date of planting shall be replaced by trees or shrubs of the same size and species and in the same place unless otherwise agreed in writing by the Local Planning Authority.

20. No part of the development hereby permitted shall be commenced until full details of hard landscape works have been submitted to and approved in writing by the District Planning Authority. The details must include existing and proposed finished levels or contours; the position, design and materials of all site enclosures, car parking layout and other vehicular and pedestrian areas; hard surfacing materials, minor artefacts and structures (e.g. street furniture, play equipment, refuse areas, lighting etc;) and any retained historic features and proposals for restoration.

The works approved as part of this condition shall be completed before the first occupation of any part of the development.

21. The development hereby approved shall not be first occupied until the signal controlled pedestrian crossing across the ring road near to the junctions of Crown Lane and Enville Street, and the pedestrian crossing to Bell Street has been provided in accordance with the approved plans.
22. The cycle parking for the site shall be provided in accordance with the approved plans, and shall be retained for the life of the development.
23. The development hereby approved shall not be first occupied until the cycle way, along Crown Lane have been provided in accordance with the approved plans. The store shall not be first used until the cycle way has been implemented.
24. The development hereby approved shall not be first commenced until details of the floor and wall treatment of the mall and food court areas, including the library entrance, has been submitted to and in writing by the Local Planning Authority. The floor and wall treatment of the mall and food court areas shall thereafter be carried out in accordance with the approved details. The store shall not first trade until the scheme has been carried out in accordance with the approved details.
25. No part of the development hereby permitted shall be commenced until samples of all external materials to be used on the walls and roofs have been submitted to and approved in writing by the Local Planning Authority and thereafter the development shall only be constructed in accordance with these approved details.
26. Notwithstanding the provisions of Part 4 of Schedule 2 of the Town and Country Planning (General Permitted Development) Order 1995 (or any order revoking or re-enacting that Order with or without modification) no buildings, compounds, structures or enclosures which are required temporarily in connection with the development hereby permitted shall be placed or erected on the site or adjacent land until details have been submitted to and approved in writing by the Local Planning Authority. Any matters covered by this condition shall thereafter only be sited in accordance with these approved details.
27. Notwithstanding the details shown on the approved plans no demolition works shall commence on the site until a phasing plan for the demolition and reconstruction of the site have been submitted to and approved in writing by the Local Planning

Authority. The works shall thereafter be carried out in accordance with the approved details, unless otherwise agreed in writing by the Local Planning Authority.

28. No demolition works shall commence until the temporary access to the library and town hall has been provided in accordance with the approved details. The temporary access shall be removed before the new store is first brought into use.
29. The development hereby approved shall not be first commenced until a walkways agreement for the mall and car park accesses or similar has been submitted to and approved in writing by the Local Planning Authority. The store shall not be first used until the walkways agreement or similar has been implemented.
30. Prior to the commencement of development approved by this planning permission (or such other date or stage in development as may be agreed in writing with the Local Planning Authority), the following components of a scheme to deal with the risks associated with contamination of the site shall each be submitted to and approved, in writing, by the local planning authority:

- 1) A preliminary risk assessment which has identified:

- all previous uses
- potential contaminants associated with those uses
- a conceptual model of the site indicating sources, pathways and receptors
- potentially unacceptable risks arising from contamination at the site.

- 2) A site investigation scheme, based on (1) to provide information for a detailed assessment of the risk to all receptors that may be affected, including those off site.

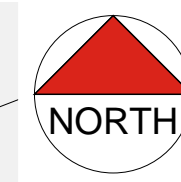
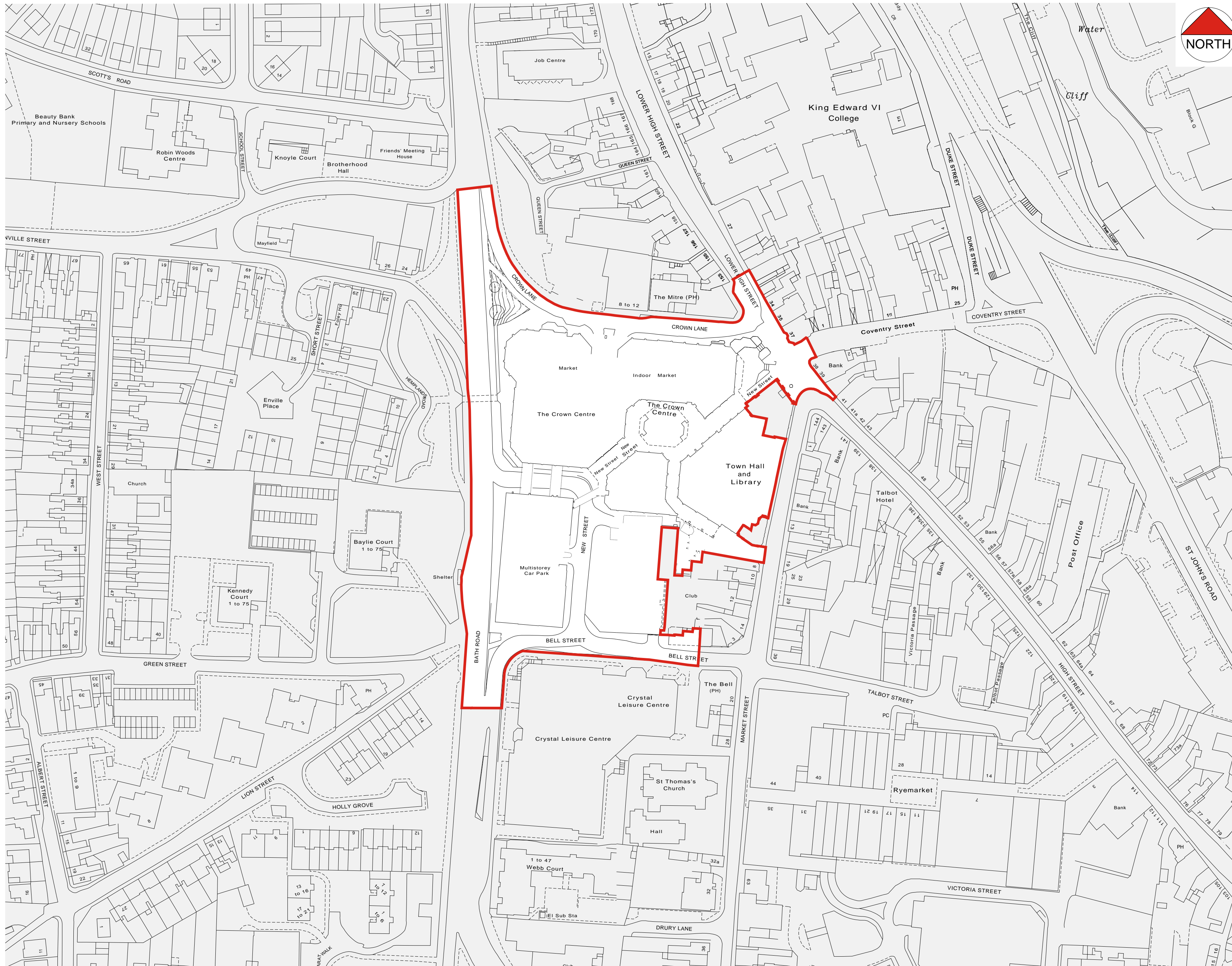
- 3) The site investigation results and the detailed risk assessment (2) and, based on these, an options appraisal and remediation strategy giving full details of the remediation measures required and how they are to be undertaken.

- 4) A verification plan providing details of the data that will be collected in order to demonstrate that the works set out in (3) are complete and identifying any requirements for longer-term monitoring of pollutant linkages, maintenance and arrangements for contingency action.

Any changes to these components require the express consent of the local planning authority. The scheme shall be implemented as approved.

31. The service area on the intermediate car parking level is retained for the life of the development.
32. Prior to the commencement of development details relating to the security measures and amended highway layout around the retained subway shall be submitted to and approved in writing by the Local Planning Authority. The works shall thereafter be carried out in accordance with the approved details and shall be completed prior to the first opening of the store.
33. Details of the Means of Escape/Pedestrian access onto the footway serving the subway will be required prior to development commencing and the agreed works completed prior to first occupation.
34. Town Hall car parking on the intermediate car parking level shall be retained for the life of the development.

35. Development shall not commence until an arrangement for the provision of traffic regulation order(s) has been submitted to and approved in writing by the Local Planning Authority. The scheme shall include the method, timing and arrangements to comply with the Council's policies for the provision of the infrastructure required in connection with the proposed development.
36. The development hereby permitted shall be carried out in accordance with the following approved plans: (TBC)



NOTES

This drawing to be read in accordance with the specification/Bills of Quantities and related drawings.  
 No Dimensions to be scaled from this drawing. All stated dimensions to be verified on site and the Architect notified of any discrepancies.

0 70  
 Scale bar 70mm at 1:1

- NOTES
- Landscaping is indicative only and to be agreed by Landscape Architect. Refer to latest Landscape Architects drawings for accurate detailed Landscape scheme
  - Highways arrangement shown on our drawings is indicative only. Latest highways design and detailed highways design is subject to agreement between Highways Authority and Highways Consultant
  - Existing levels have been taken from Ordnance Survey and Topographical Survey information by others
  - Proposed levels to be confirmed by Structural and Civil Engineer and to be agreed with client. Proposed levels subject to change following any remediation work carried out to the site after our drawings have been issued in which instance we advise a revised Topographical Survey is carried out.

LEGEND	
	Planning Application Boundary

REV.	DATE	NOTE	IN

Project

**PROPOSED RETAIL DEVELOPMENT  
 CROWN CENTRE  
 STOURBRIDGE**

Title

**PLANNING APPLICATION  
 BOUNDARY**

Scale  
 1:1250 @A2

Date  
 SEPT 10

Drawn  
 ZM

Checked

Drawing Number  
**6376 - P100.0**

Revision  
 -

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**PLANNING APPLICATION BOUNDARY**





NOTES

This drawing to be read in accordance with the specification/Bills of Quantities and related drawings.

No Dimensions to be scaled from this drawing. All stated dimensions to be verified on site and the Architect notified of any discrepancies.

0 100  
Scale bar 100mm at 1:1

NOTES

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Project  
**PROPOSED RETAIL DEVELOPMENT  
 CROWN CENTRE  
 STOURBRIDGE**

Title  
**EXISTING BLOCK PLAN**

Scale  
**1:500 @A2**

Date  
**OCTOBER 10**

Drawn  
**AC**

Checked  
**MW**

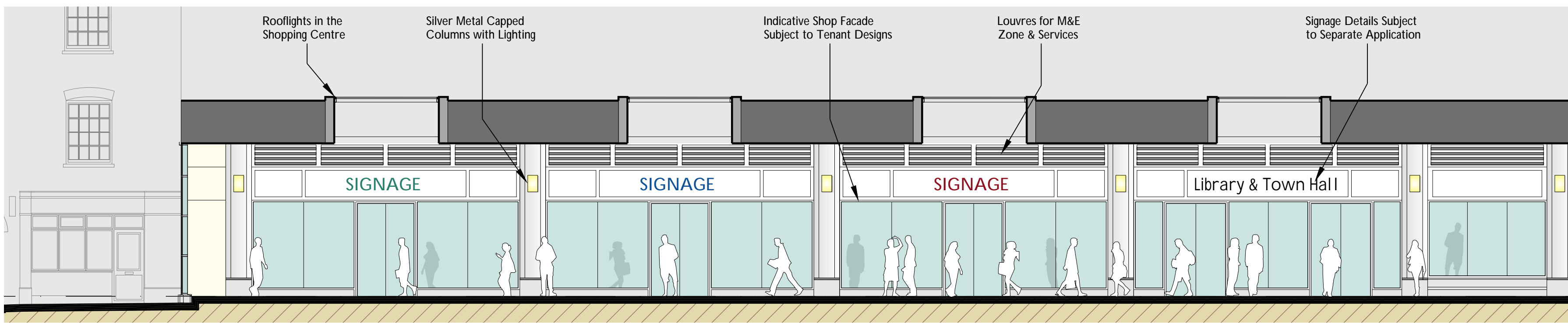
Drawing Number  
**6376 - P135.0**

Revision  
**-**

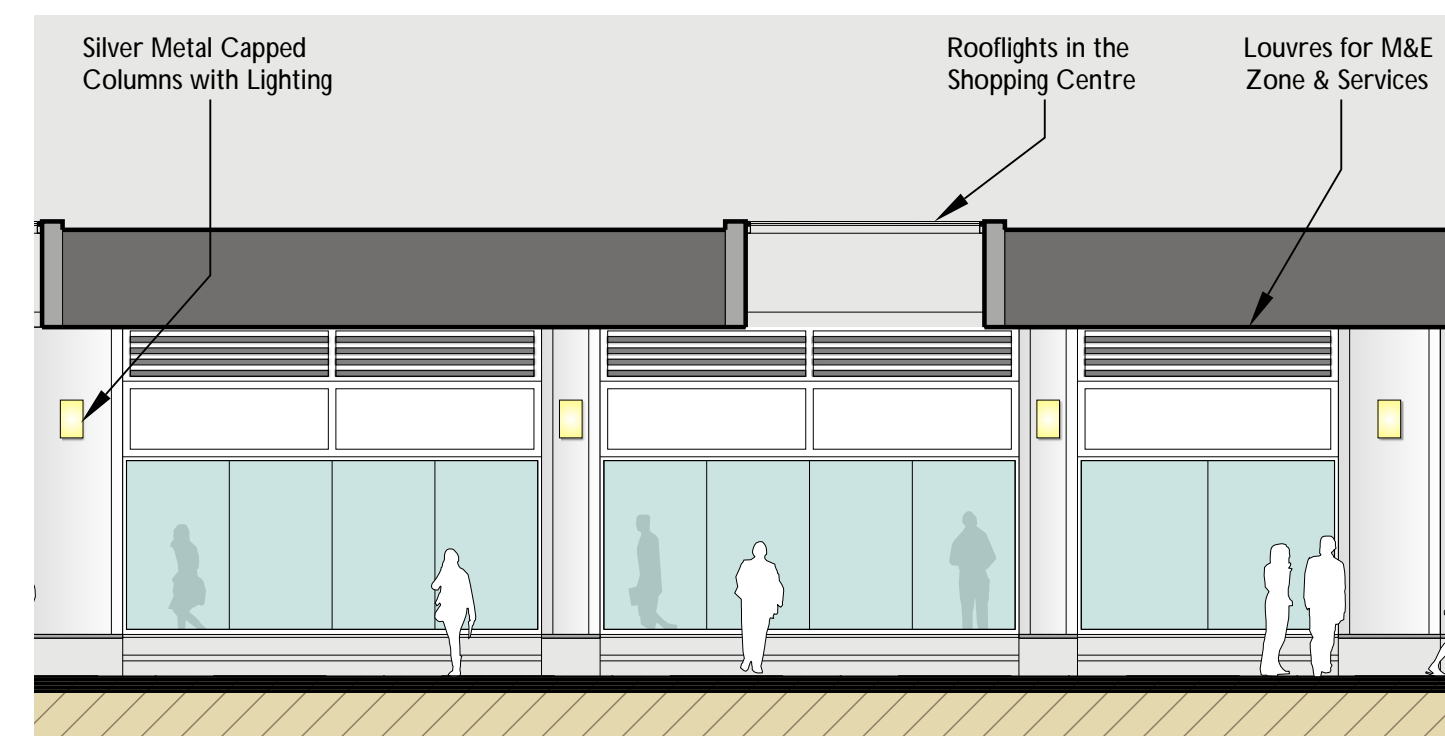
Saunders Partnership Architects  
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 Hertfordshire, AL7 3AX

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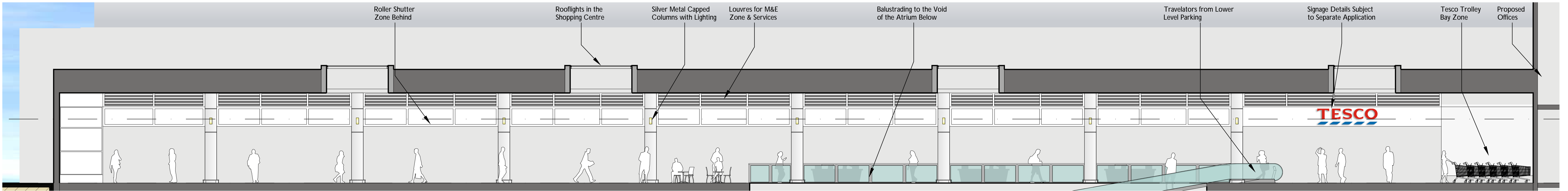
VIEW 1 - NEW RETAIL ELEVATION



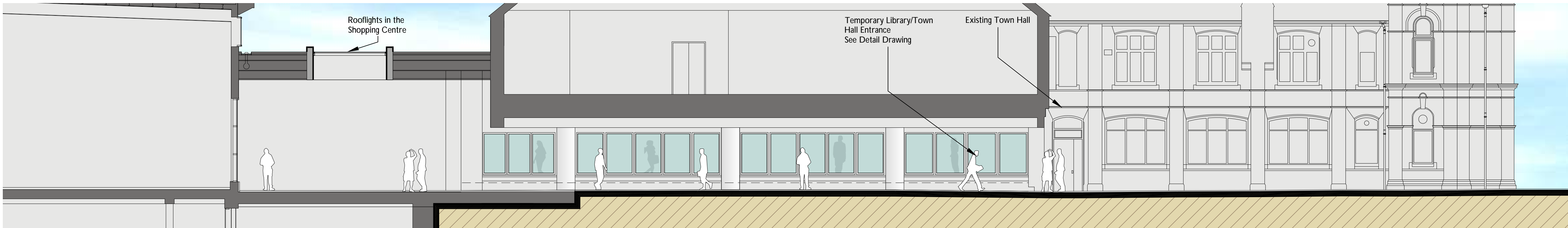
VIEW 2 - NEW RETAIL ELEVATION

NOTES  
 This drawing to be read in accordance with the specification/Bills of Quantities and related drawings.  
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 Scale bar 100mm at 1:1

ELEVATIONS SHOWN INDICATIVELY SUBJECT TO TENANT AGREEMENT

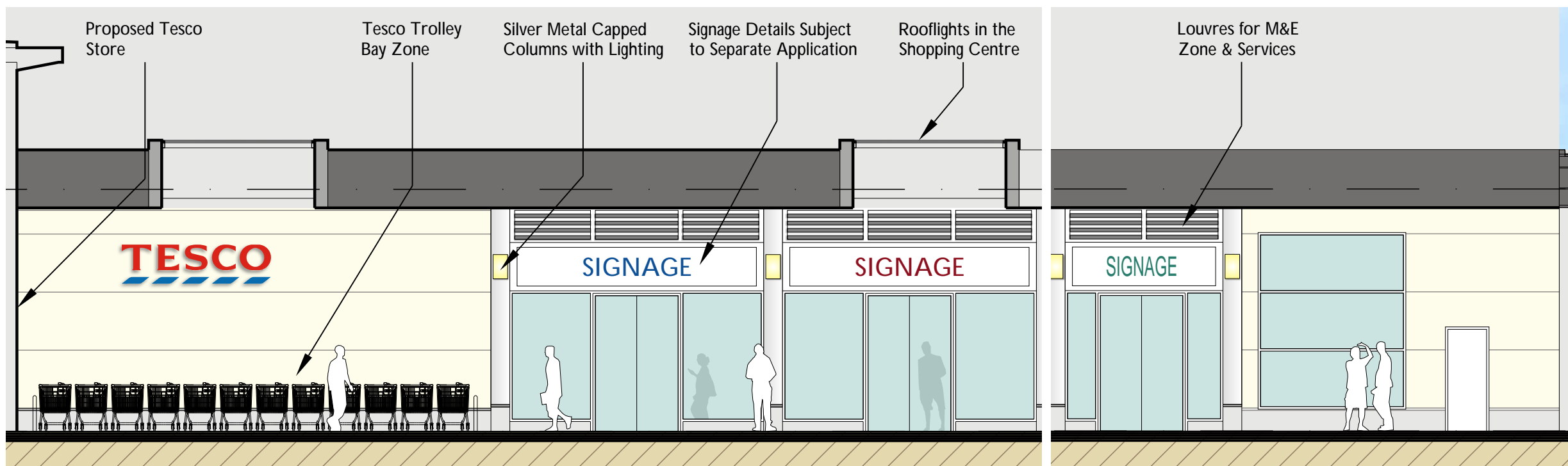


VIEW 3 - TESCO MALL ELEVATION

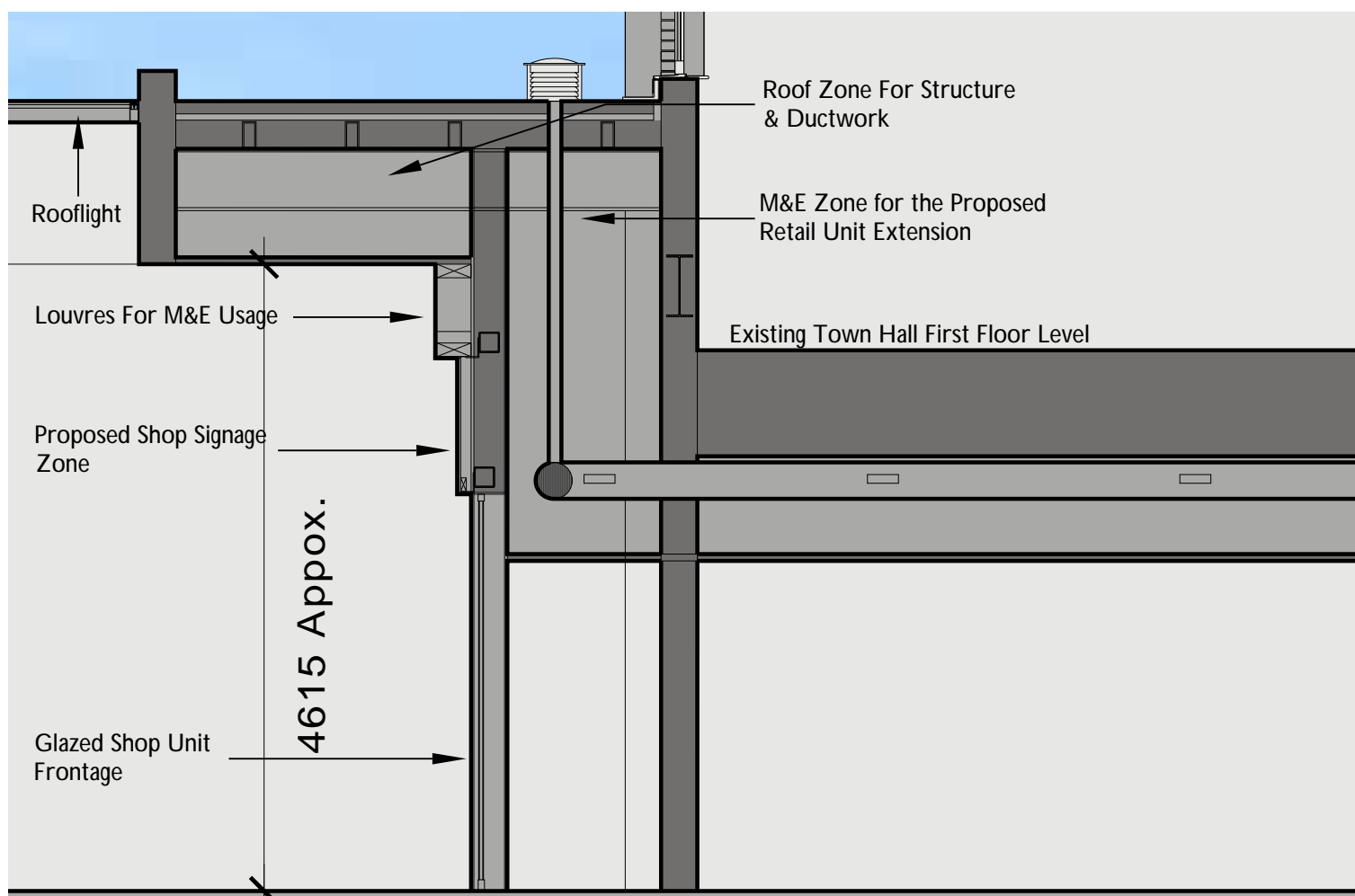


VIEW 4 - NEW TOWN HALL/LIBRARY ENTRANCE

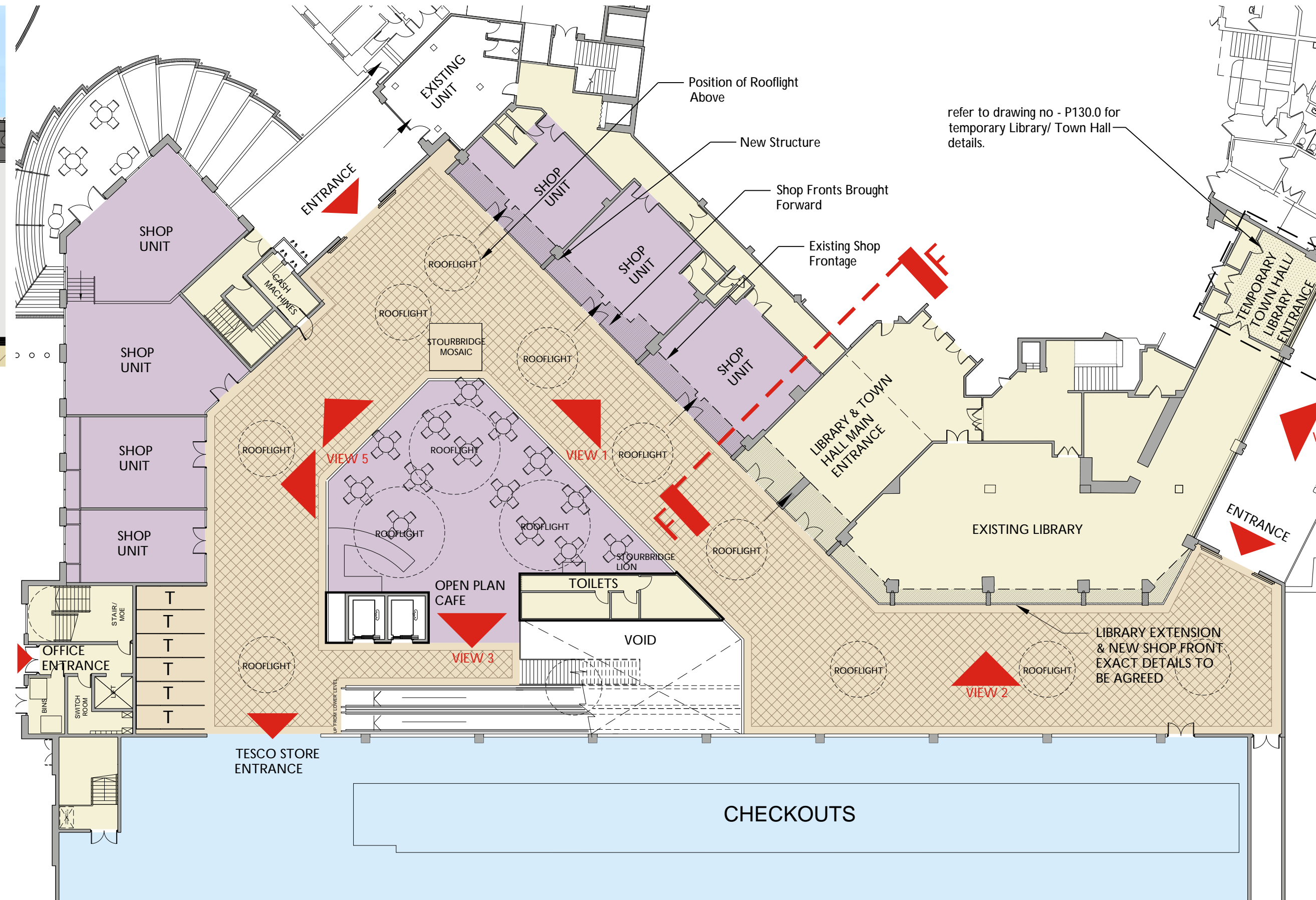
- Temporary proposed works to Town Hall
- Town Hall/Library entrance closed and temporary entrance built from Market Street.
  - New Temporary means of escape door added into the lobby of the town hall to replace the closing of the entrance doors during construction.
  - managers office to be relocated to an existing small shop unit on Market Street to allow for temporary entrance.
- Permenant proposed works to Town Hall
- Existing brick piers extended to form new frontage to library and shop units.
  - re clad new shop and library fronage.
  - carpets in library and townhall extension to be made good/replaced where required.
  - Existing offices attached to library to be demolished. Make good junction between proposed external skin and existing elements retained.



VIEW 5 - NEW RETAIL & OFFICE ELEVATION



RETAIL UNIT SECTION F-F (SCALE 1:50)



MALL PLAN (SCALE 1:250)

REV.	DATE	NOTE	IN

Project  
**PROPOSED RETAIL DEVELOPMENT CROWN CENTRE STOURBRIDGE**

Title  
**PROPOSED MALL ELEVATIONS**

Scale 1:100 @ A1 Date SEPT 10  
 Drawn JKS Checked

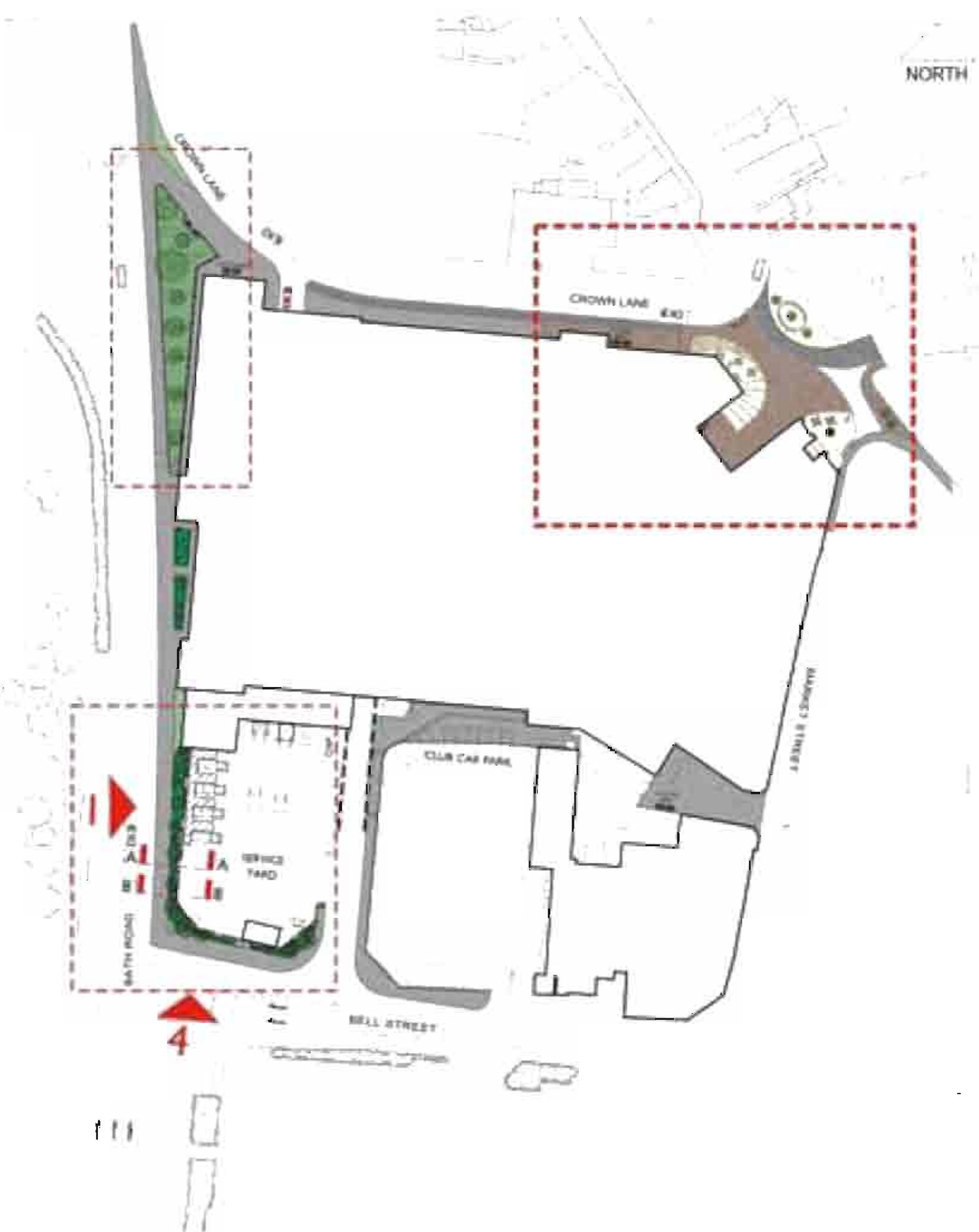
Drawing Number 6376 - P114.0 Revision -

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 Hertfordshire, AL7 3AX

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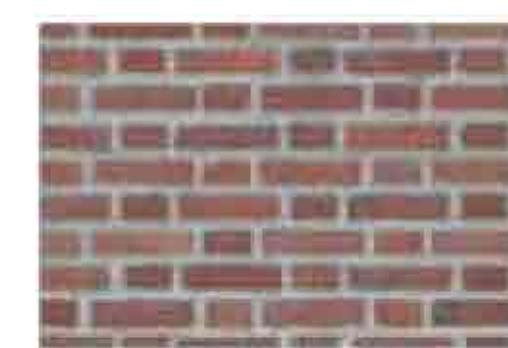
OVERALL KEY PLAN SCALE 1:1000



PROPOSED SECTION A-A SCALE 1:100



PROPOSED SECTION B-B SCALE 1:100



F- RED BRICK WALL



E- STONE CAPPING



G- GABION WALL



G- WALL PLANTING



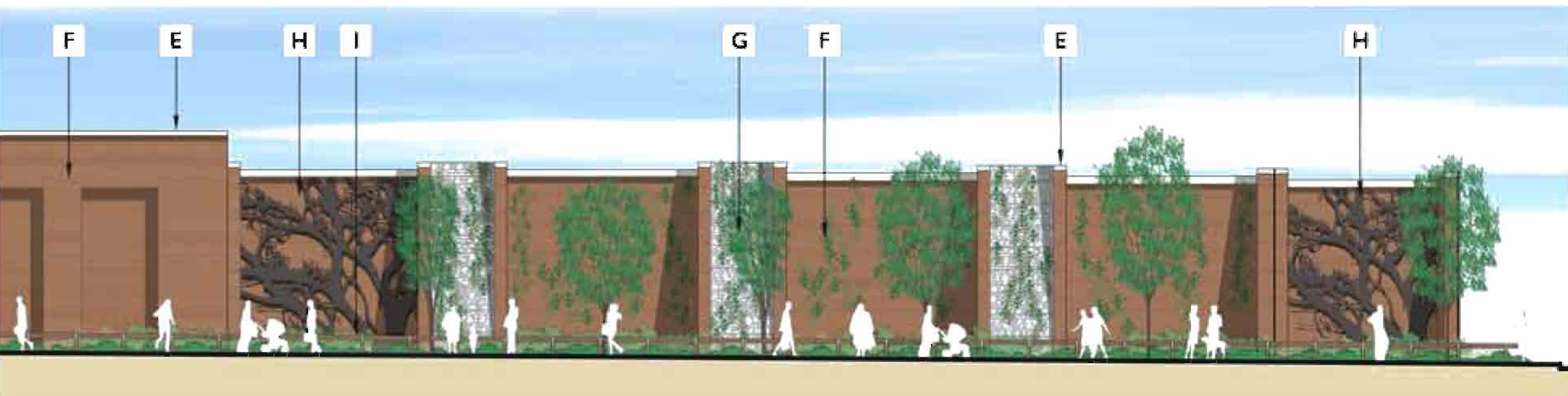
H- DECORATIVE STEEL SCULPTURAL PANEL

NOTES  
This drawing to be read in accordance with the specifications/Bills of Materials and related drawings.  
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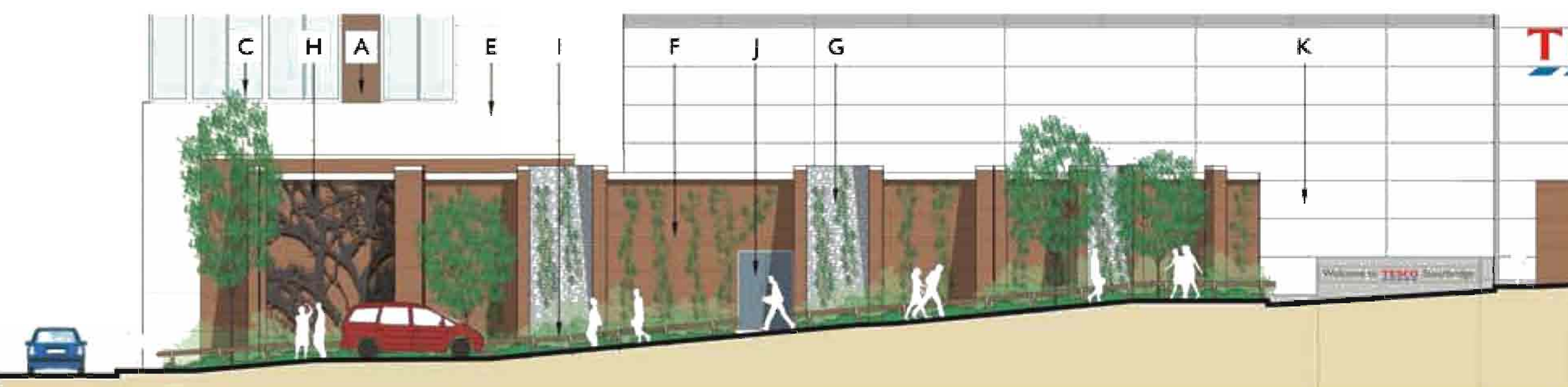
NOTES  
Landscaping is indicative only and to be agreed by Landscape Architect Refer to latest Landscape Architects drawings for accurate detailed Landscape scheme.  
Highways arrangements shown on our drawings is indicative only. Latest Highways design and detailed Highways design is subject to agreement between Highways Authority and Highways Consultant.  
Existing levels have been taken from Ordnance Survey and Topographical Survey information by others.  
Proposed levels to be confirmed by Structural and Civil Engineer and to be agreed with client. Proposed levels subject to change following any reposition work carried out to the site after our drawings have been issued in which instance we advise a revised Topographical Survey is carried out.

LEGEND

	Tarmac to match existing
	Tarmac Cycle Track
	Red Herring Block Paving
	Natural Paving
	Existing trees retained
	Proposed trees



PROPOSED ELEVATION VIEW 1 SCALE 1:100



PROPOSED ELEVATION VIEW 4 SCALE 1:100

MATERIALS

A	Timber Larch Panels
B	Composite Cladding Panel (Blue/Dark Grey)
C	Glazing
D	Kingspan KS1000TD Roof Panel
E	Reconstituted Stone Walling with Smooth Joints
F	Red Brickwork Wall
G	Gabion Walling to allow Plant Growth
H	Decorative Steel Sculptural Panel in Abstract 'Tree' Design, Machine cut and powder coated black with matt finish (iron appearance)
I	Timber Knee Rail
J	PPC Doors (Colour to match cladding panels)
K	Composite Cladding Panel (Light Silver)
L	Louvers to concealed plant area behind

Project  
**PROPOSED RETAIL DEVELOPMENT  
CROWN CENTRE  
STOURBRIDGE**

Title  
**PROPOSED  
SERVICE YARD WALL DETAILS**



DECORATIVE STEEL SCULPTURAL PANEL

Scale  
Varies @A1

Date  
SEPT 10

Drawn  
ZM

Checked

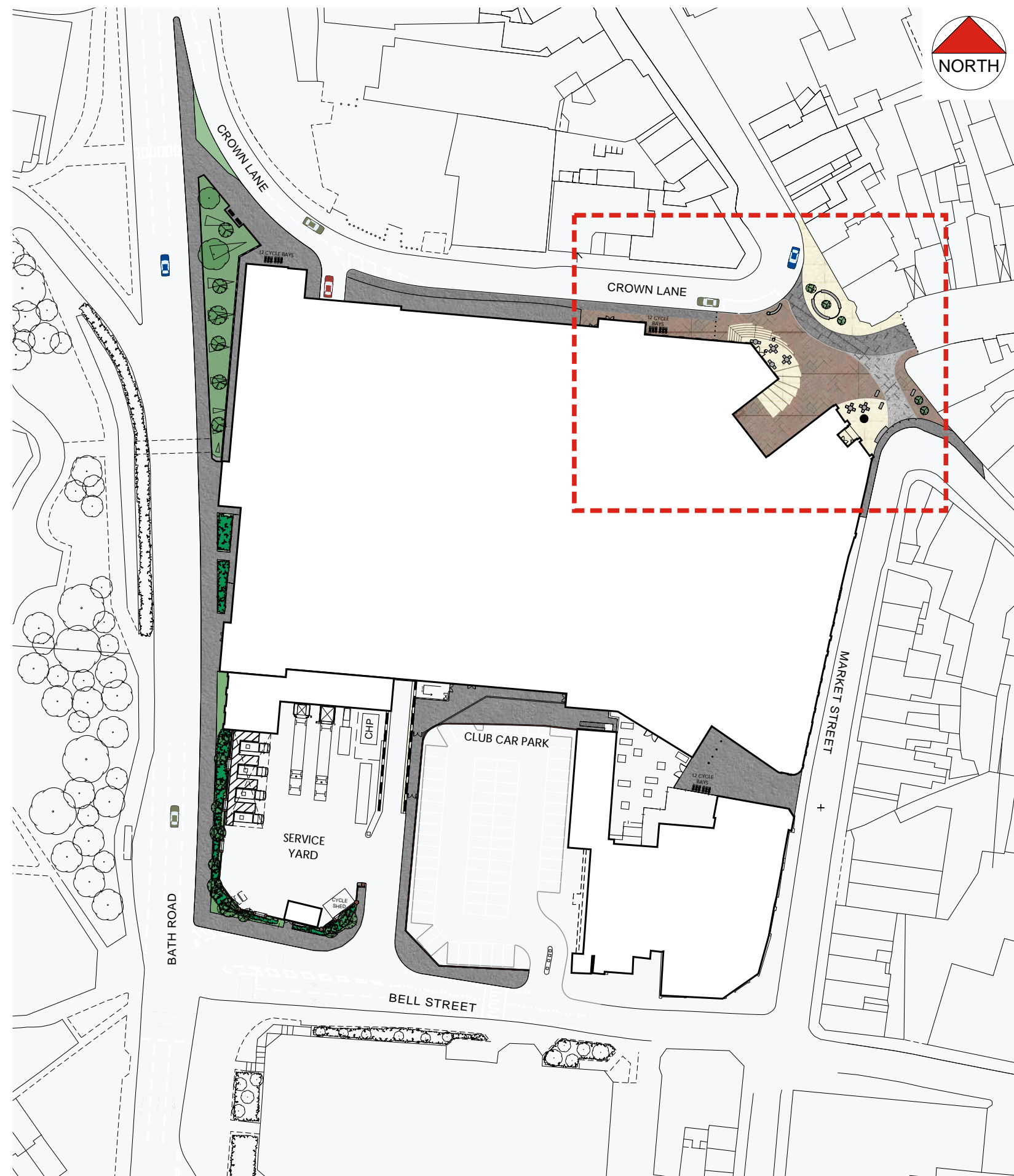
Drawing Number  
**6376 - P109.0**

Revision  
-

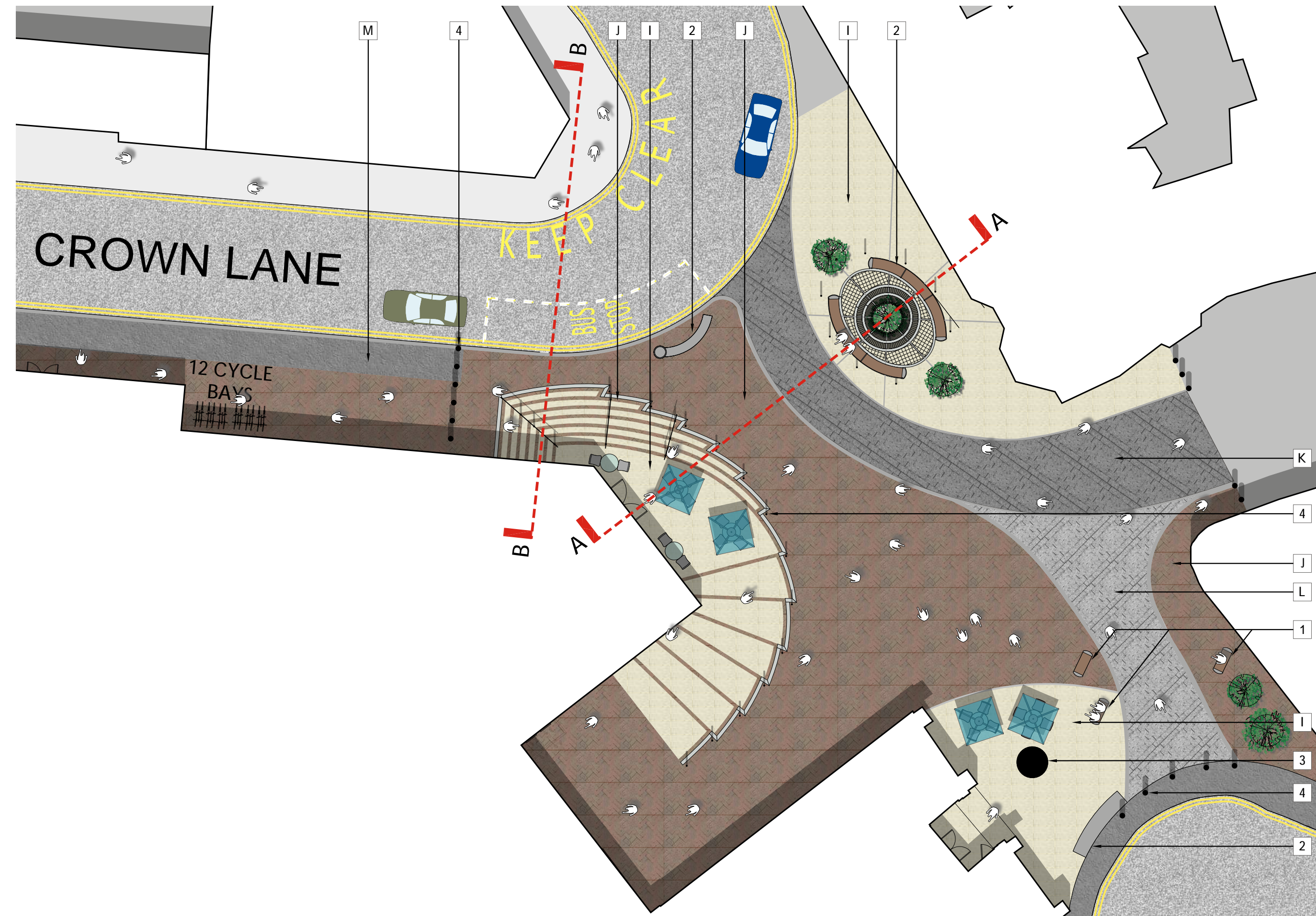
Saunders Partnership Architects  
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OVERALL KEY PLAN SCALE 1:1000



PUBLIC REALM SCALE 1:200



1- WOODHOUSE GEO TIMBER BENCH (1800 x 675 x 440mm)



2- WOODHOUSE TIMBER CURVED SEAT

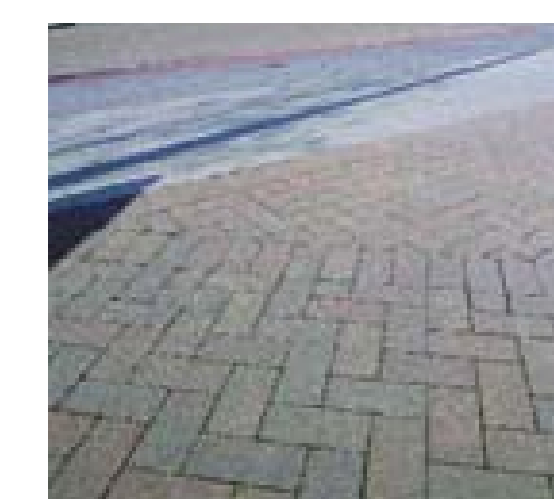


4- WOODHOUSE GEO STAINLESS STEEL BOLLARDS (140mm)



I- BUFF PAVING

J- RED HERRING BLOCK PAVING  
K- CHARCOAL PAVING (ALL MARSHALLS)

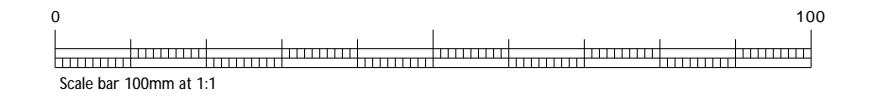


J- RED HERRINGBONE PATTERN BLOCK PAVING  
K- CHARCOAL PAVING (ALL MARSHALLS)

NOTES

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LEGEND

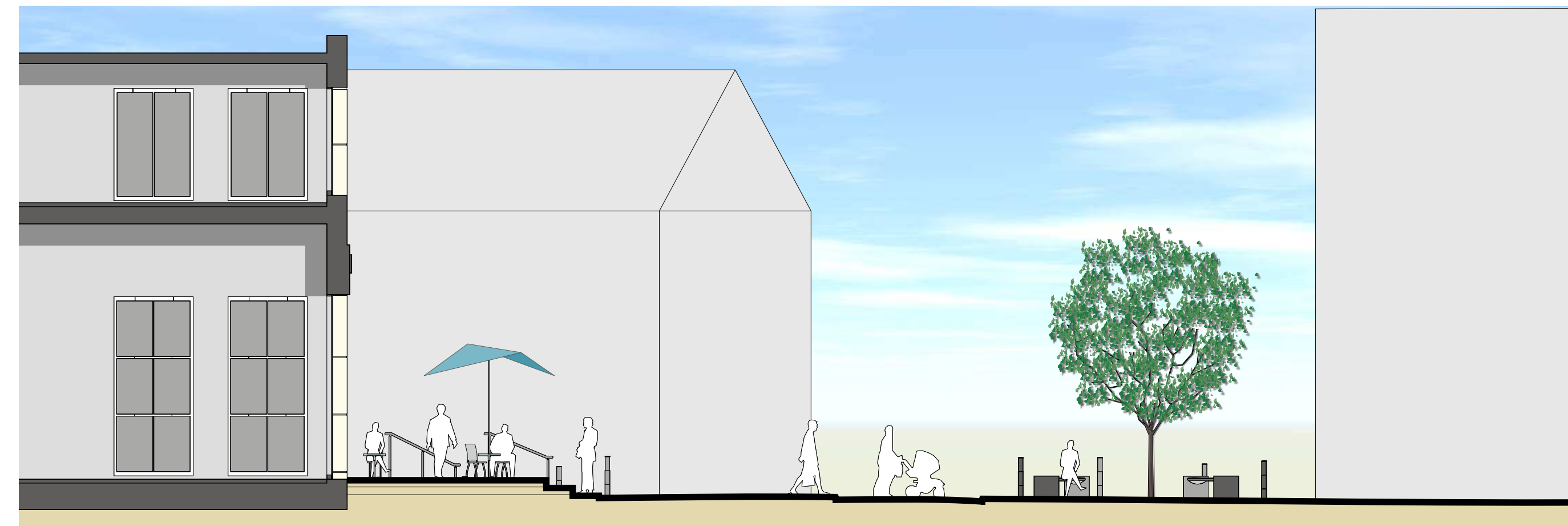
	Tarmac to match existing
	Tarmac Cycle Track
	Red Herring Block Paving
	Buff Natural Paving
	Existing trees retained
	Proposed trees

MATERIALS

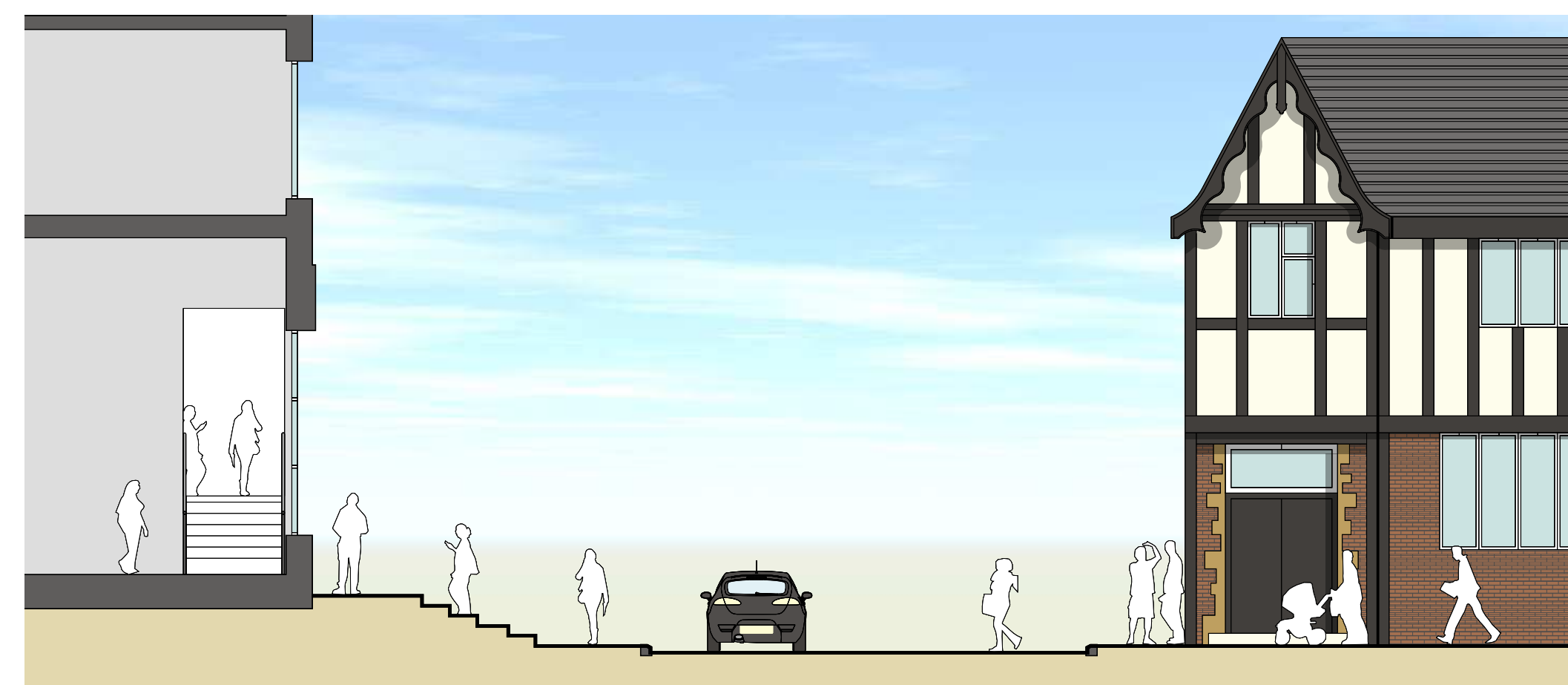
A	Timber Larch Panels
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E	Reconstituted Stone Walling with Smooth Joints
F	Red Brickwork Wall
G	Gabion Walling to allow Plant Growth
H	Decorative Steel Sculptural Panel in Abstract 'Tree' Design
I	Buff Natural Paving
J	Red Herring Block Paving
K	Charcoal Paving
L	Tarmac to match existing
M	Tarmac Cycle Path

COMPONENTS

1	Woodhouse GEO Bench (1800 x 675 x 440mm)
2	Woodhouse Curved Seat
3	Existing Clock Tower Retained
4	Woodhouse Geo Stainless steel Bollards (1400mm)



PROPOSED SECTION A-A SCALE 1:100



PROPOSED SECTION B-B SCALE 1:100

Project

PROPOSED RETAIL DEVELOPMENT  
CROWN CENTRE  
STOURBRIDGE

Title

PROPOSED  
EXTERNAL FINISHES

Scale @ A1 Date AUGUST 10

Drawn ZM Checked

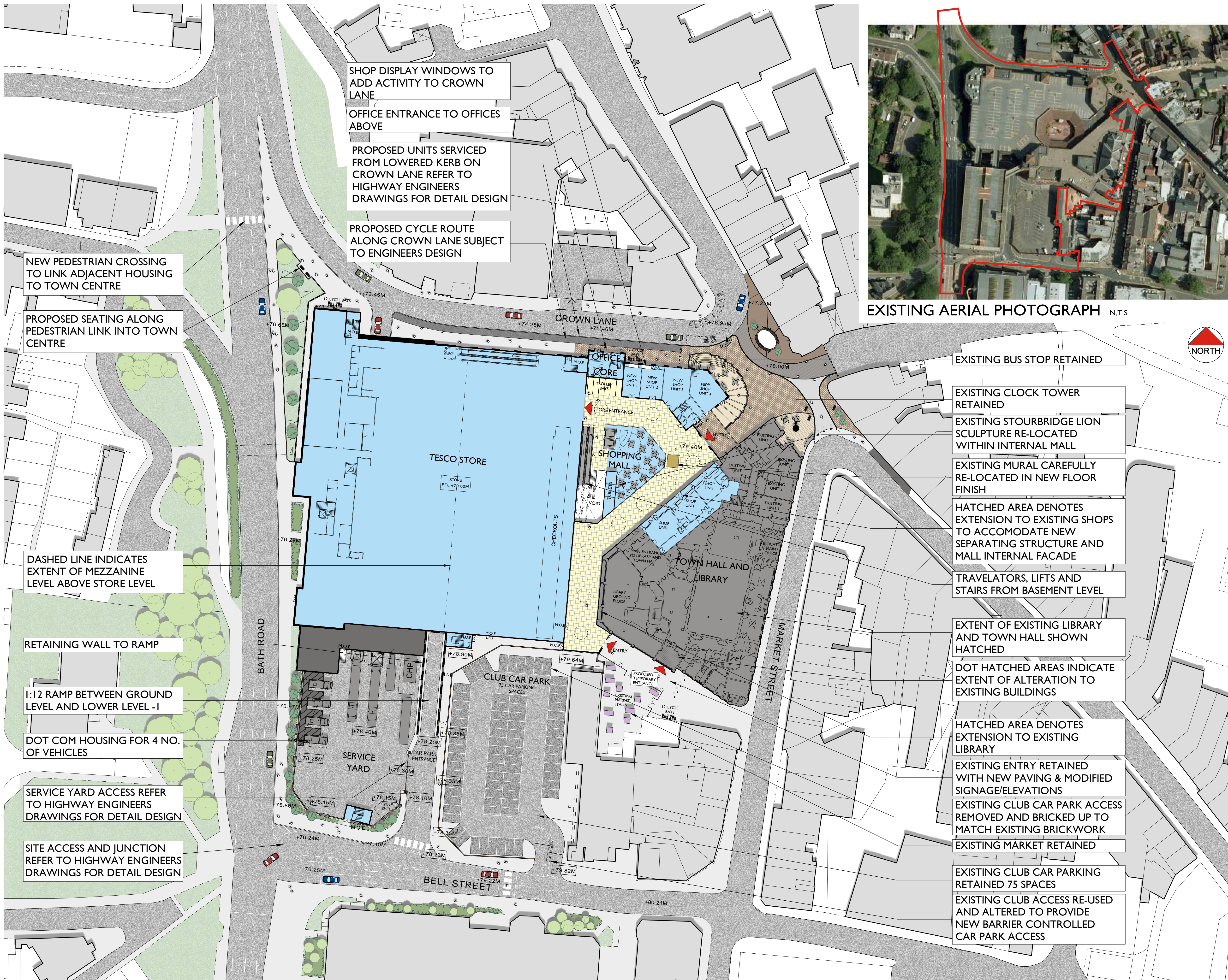
Drawing Number 6376 - P108.1 Revision -

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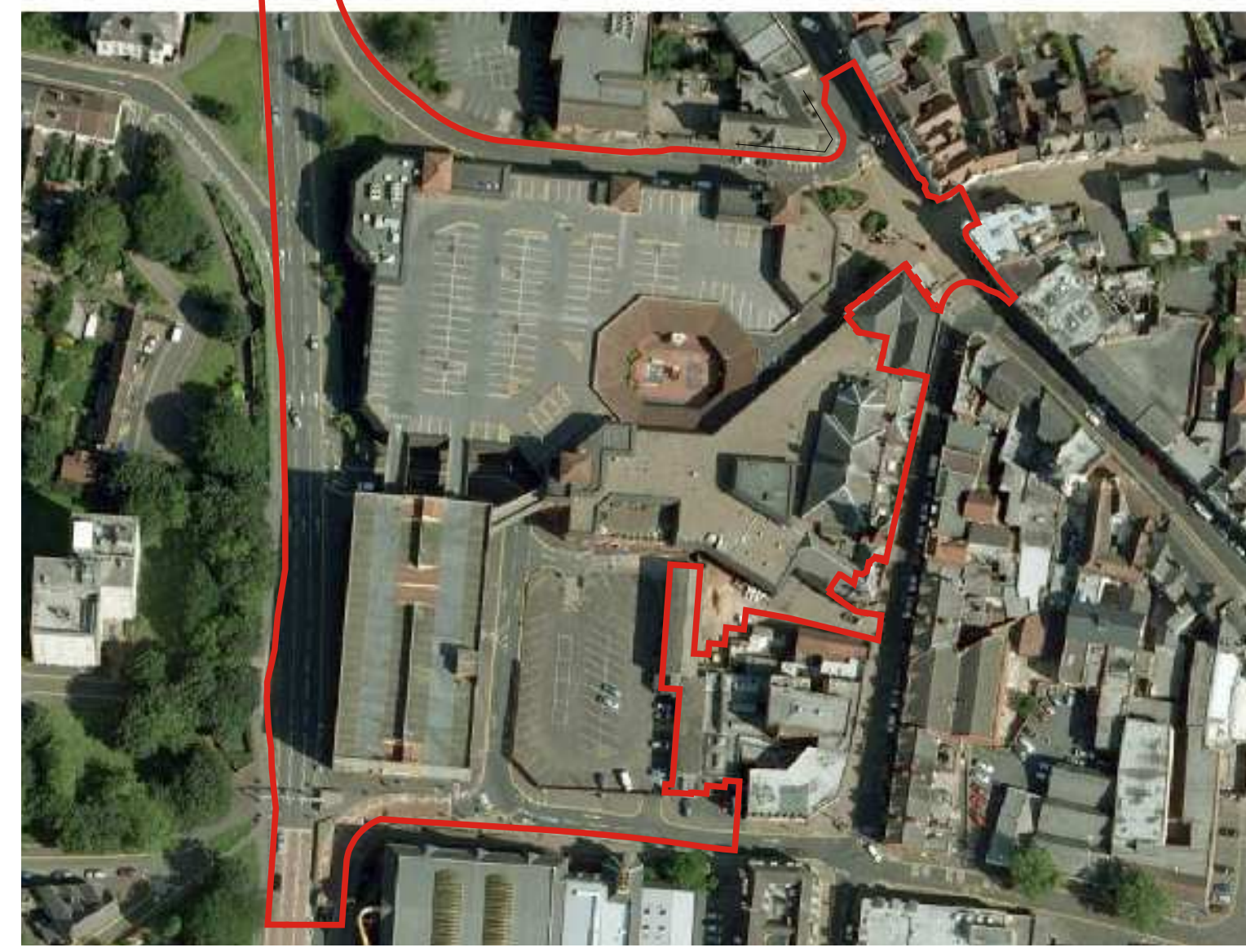
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PROPOSED GROUND FLOOR PLAN LEVEL 0



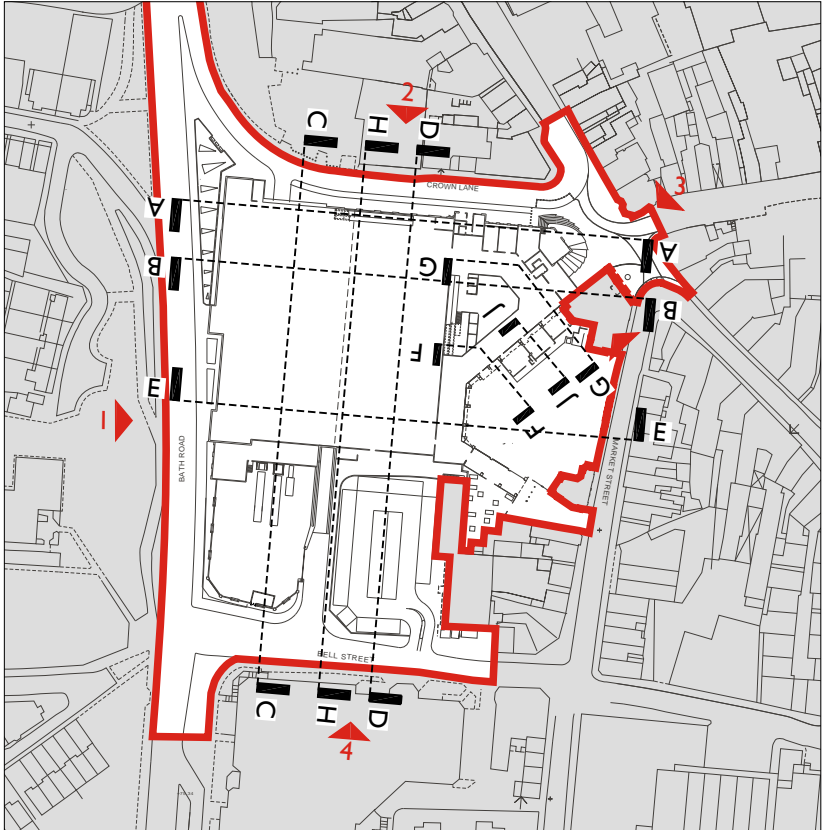
EXISTING AERIAL PHOTOGRAPH N.T.S

NOTES  
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 0 100  
 Scale bar 100mm at 1:1

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 INTERNAL LAYOUT FOR ILLUSTRATIVE PURPOSES ONLY

LEGEND

	Site Boundary
	Proposed Retaining Wall
	Existing TPO trees retained
	Proposed Trees
+00.00M	Existing Levels
+00.00M	Proposed Levels



KEY PLAN SCALE 1:2500

REV.	DATE	NOTE	IN

Project  
**PROPOSED RETAIL DEVELOPMENT  
 CROWN CENTRE  
 STOURBRIDGE**

Title  
**PROPOSED  
 GROUND FLOOR PLAN  
 LEVEL 0**

Scale 1:500 @A1 Date SEPT 10  
 Drawn LC Checked

Drawing Number 6376 - P101.2 Revision

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NEW PEDESTRIAN CROSSING TO LINK ADJACENT HOUSING TO TOWN CENTRE

PROPOSED SEATING ALONG PEDESTRIAN LINK INTO TOWN CENTRE

DASHED LINE INDICATES EXTENT OF MEZZANINE LEVEL ABOVE STORE LEVEL

RETAINING WALL TO RAMP

1:12 RAMP BETWEEN GROUND LEVEL AND LOWER LEVEL -1

DOT COM HOUSING FOR 4 NO. OF VEHICLES

SERVICE YARD ACCESS REFER TO HIGHWAY ENGINEERS DRAWINGS FOR DETAIL DESIGN

SITE ACCESS AND JUNCTION REFER TO HIGHWAY ENGINEERS DRAWINGS FOR DETAIL DESIGN

SHOP DISPLAY WINDOWS TO ADD ACTIVITY TO CROWN LANE

OFFICE ENTRANCE TO OFFICES ABOVE

PROPOSED UNITS SERVICED FROM LOWERED KERB ON CROWN LANE REFER TO HIGHWAY ENGINEERS DRAWINGS FOR DETAIL DESIGN

PROPOSED CYCLE ROUTE ALONG CROWN LANE SUBJECT TO ENGINEERS DESIGN

EXISTING BUS STOP RETAINED

EXISTING CLOCK TOWER RETAINED

EXISTING STOURBRIDGE LION SCULPTURE RE-LOCATED WITHIN INTERNAL MALL

EXISTING MURAL CAREFULLY RE-LOCATED IN NEW FLOOR FINISH

HATCHED AREA DENOTES EXTENSION TO EXISTING SHOPS TO ACCOMMODATE NEW SEPARATING STRUCTURE AND MALL INTERNAL FACADE

TRAVELATORS, LIFTS AND STAIRS FROM BASEMENT LEVEL

EXTENT OF EXISTING LIBRARY AND TOWN HALL SHOWN HATCHED

DOT HATCHED AREAS INDICATE EXTENT OF ALTERATION TO EXISTING BUILDINGS

HATCHED AREA DENOTES EXTENSION TO EXISTING LIBRARY

EXISTING ENTRY RETAINED WITH NEW PAVING & MODIFIED SIGNAGE/ELEVATIONS

EXISTING CLUB CAR PARK ACCESS REMOVED AND BRICKED UP TO MATCH EXISTING BRICKWORK

EXISTING MARKET RETAINED

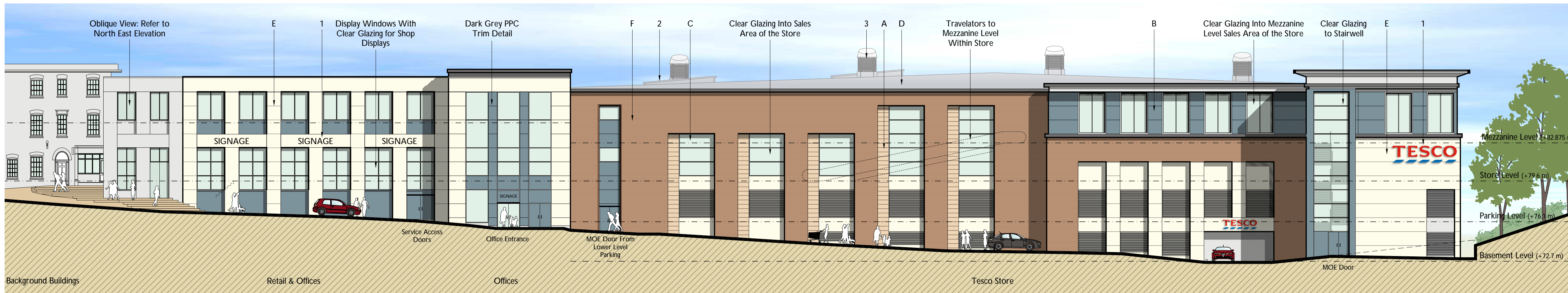
EXISTING CLUB CAR PARKING RETAINED 75 SPACES

EXISTING CLUB ACCESS RE-USED AND ALTERED TO PROVIDE NEW BARRIER CONTROLLED CAR PARK ACCESS

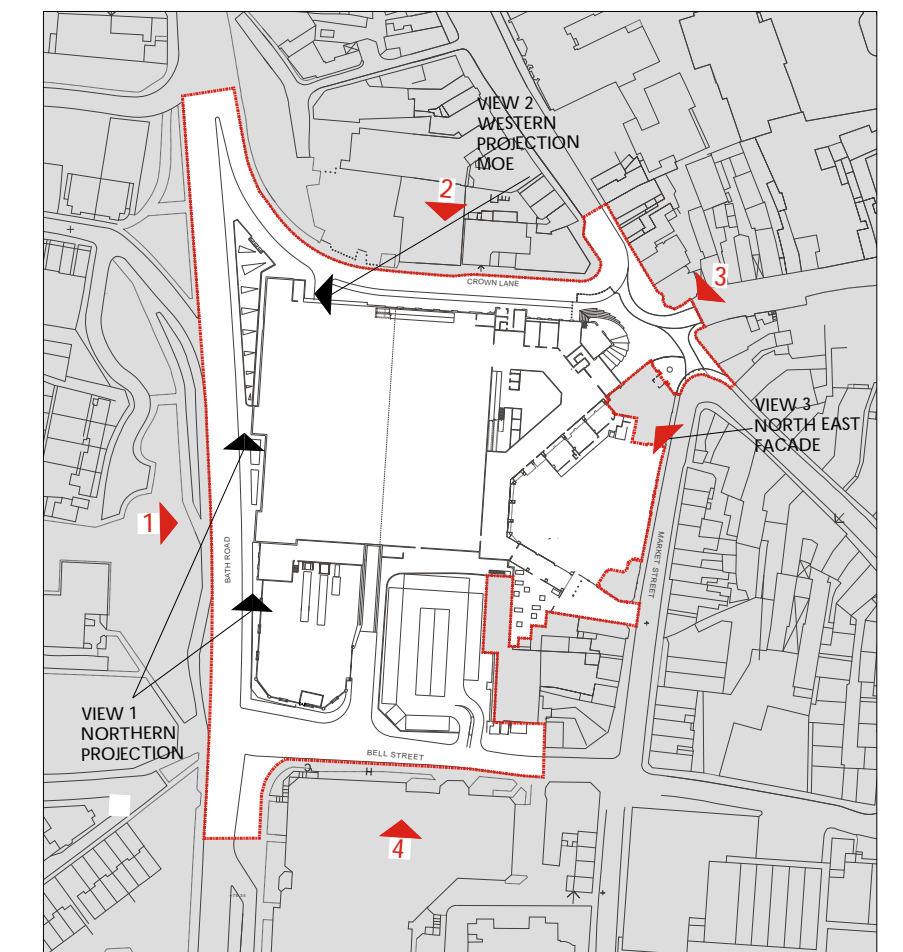




VIEW 1 - WEST ELEVATION



VIEW 2 - NORTH ELEVATION



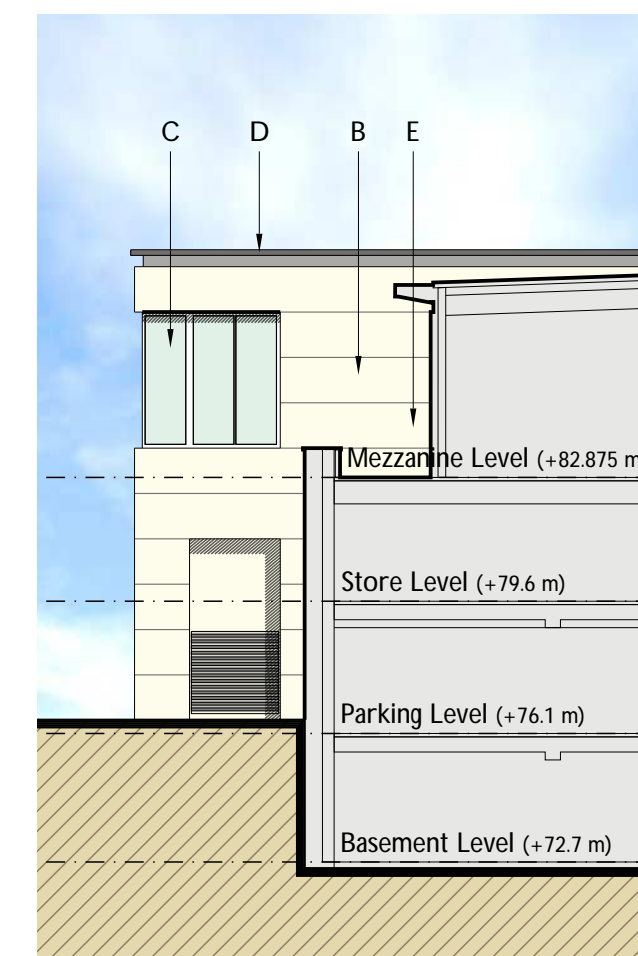
KEY PLAN



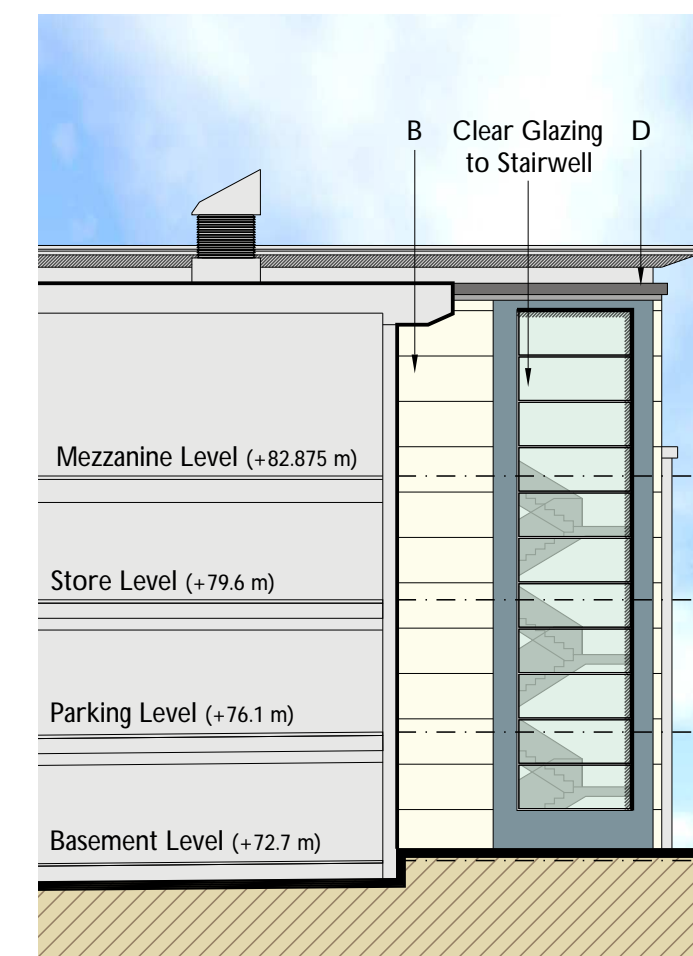
VIEW 3 - NORTH EAST ELEVATION



VIEW 3 - NORTH EAST FACADE



VIEW 1 - NORTHERN PROJECTION



VIEW 2 - WESTERN PROJECTION (MOE)

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**MATERIALS**

A	Timber Larch Panels
B	Composite Cladding Panel (Blue/Dark Grey)
C	Glazing
D	Kingspan KS1000TD Roof Panel
E	Reconstituted Stone Panel Walling with Smooth Joints
F	Red Brickwork Wall

**COMPONENTS**

1	Signage shown indicative Subject to separate Application
2	Xtraight Rooflight for Natural Daylight
3	Monodraught Windcatcher for Natural Ventilation

Project  
**PROPOSED RETAIL DEVELOPMENT  
 CROWN CENTRE  
 STOURBRIDGE**

Title  
**PROPOSED ELEVATIONS**



VIEW 4 - SOUTH ELEVATION

Scale  
 1:200 @A1  
 Date  
 SEPT 10

Drawn  
 LC  
 Checked

Drawing Number  
**6376 - P103.0**

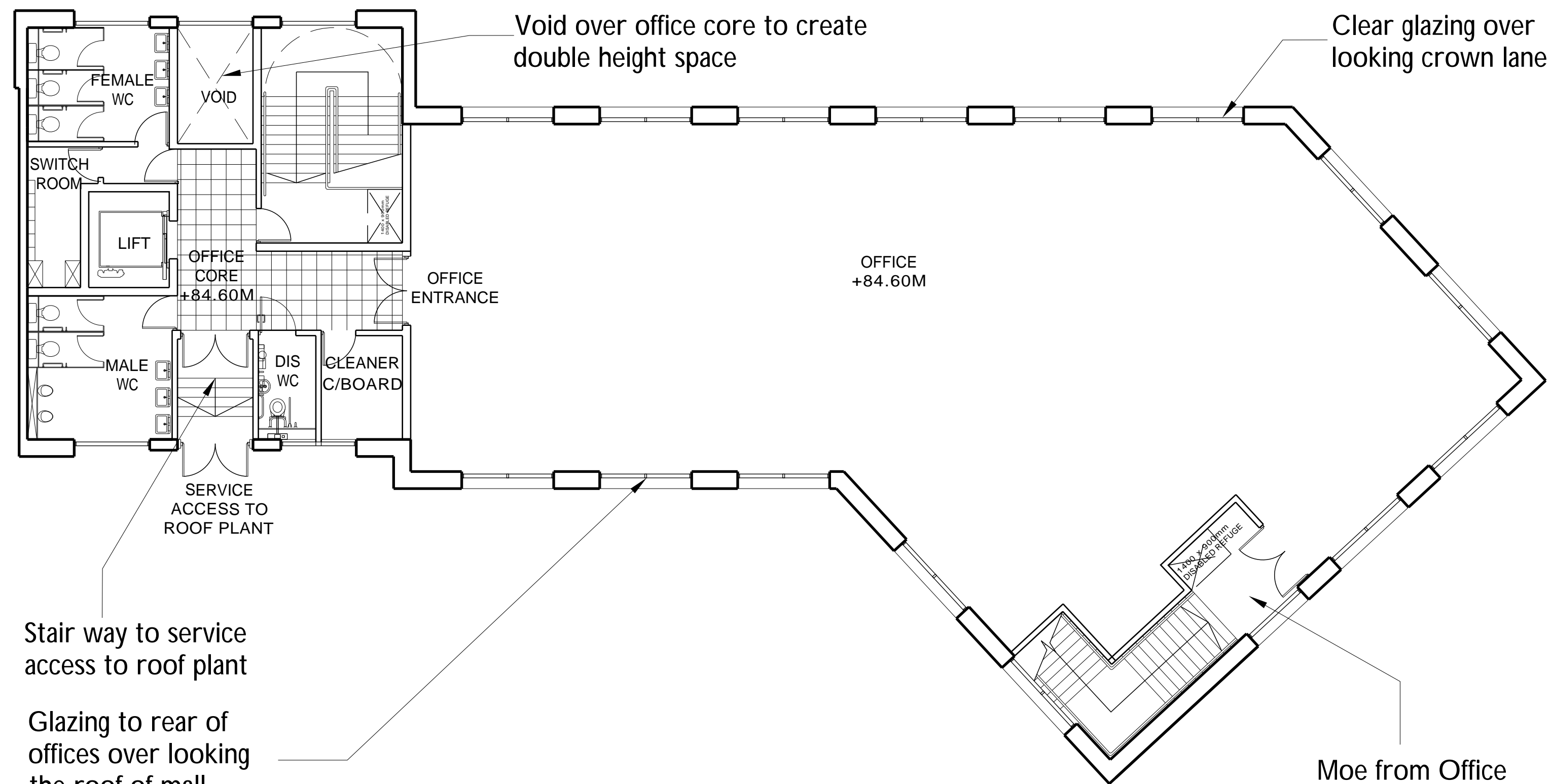
Revision  
 -

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 Hertfordshire, AL7 3AX

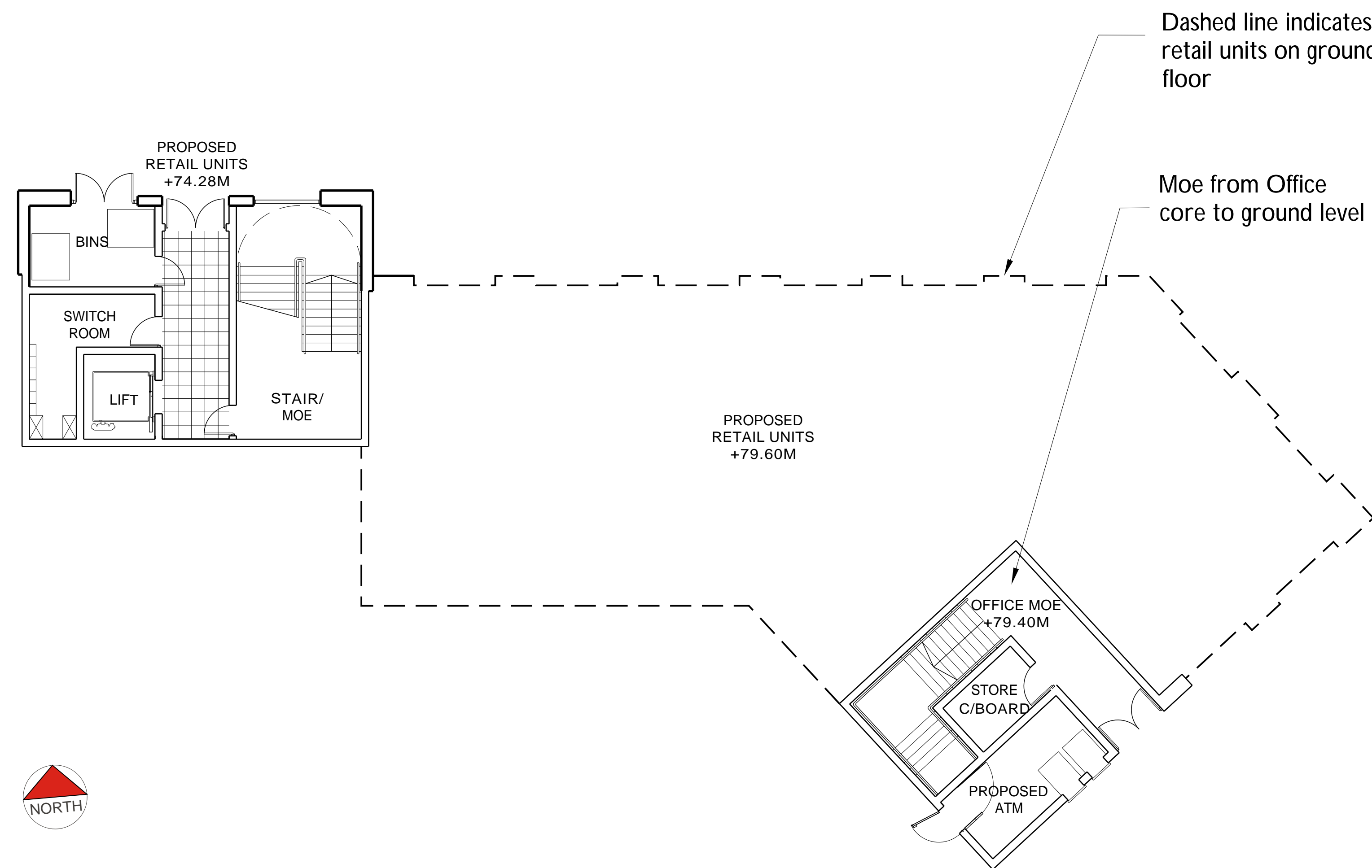
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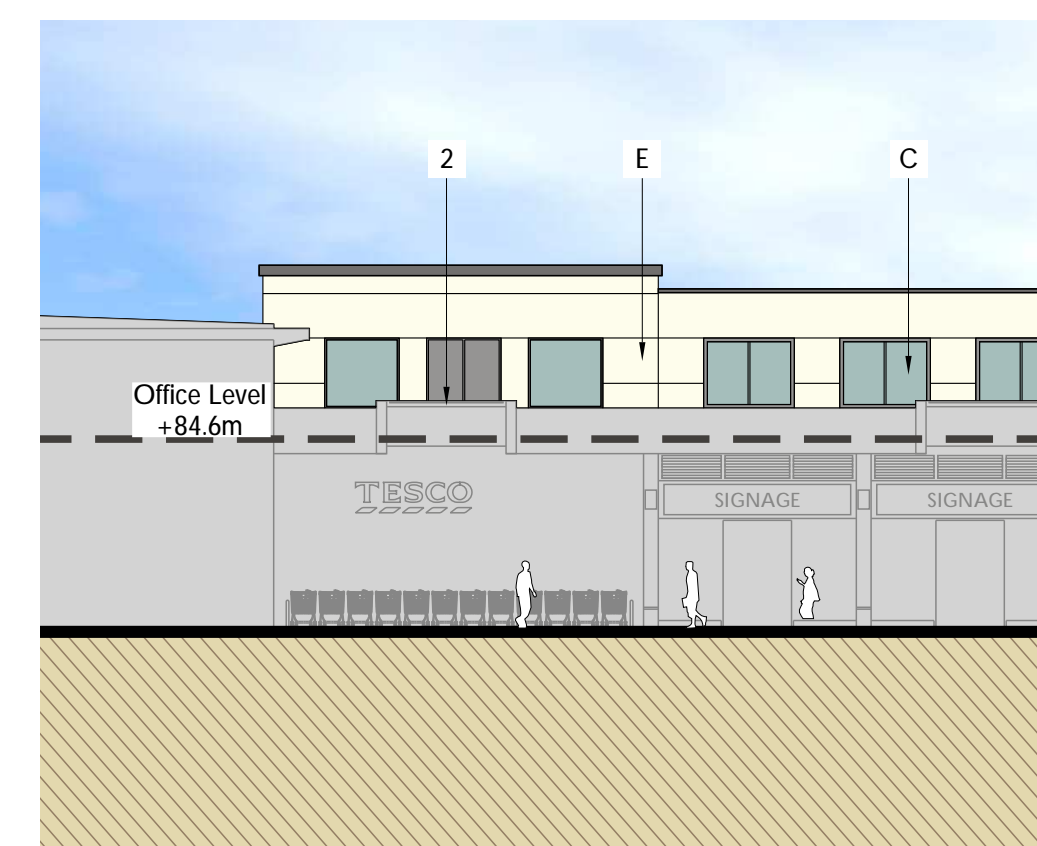




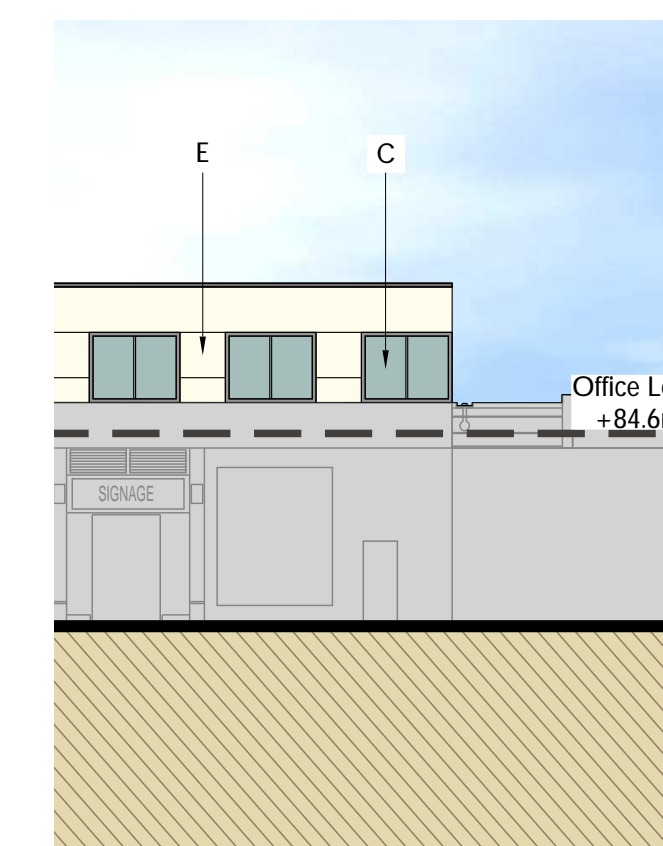
PROPOSED FIRST FLOOR PLAN SCALE 1:100



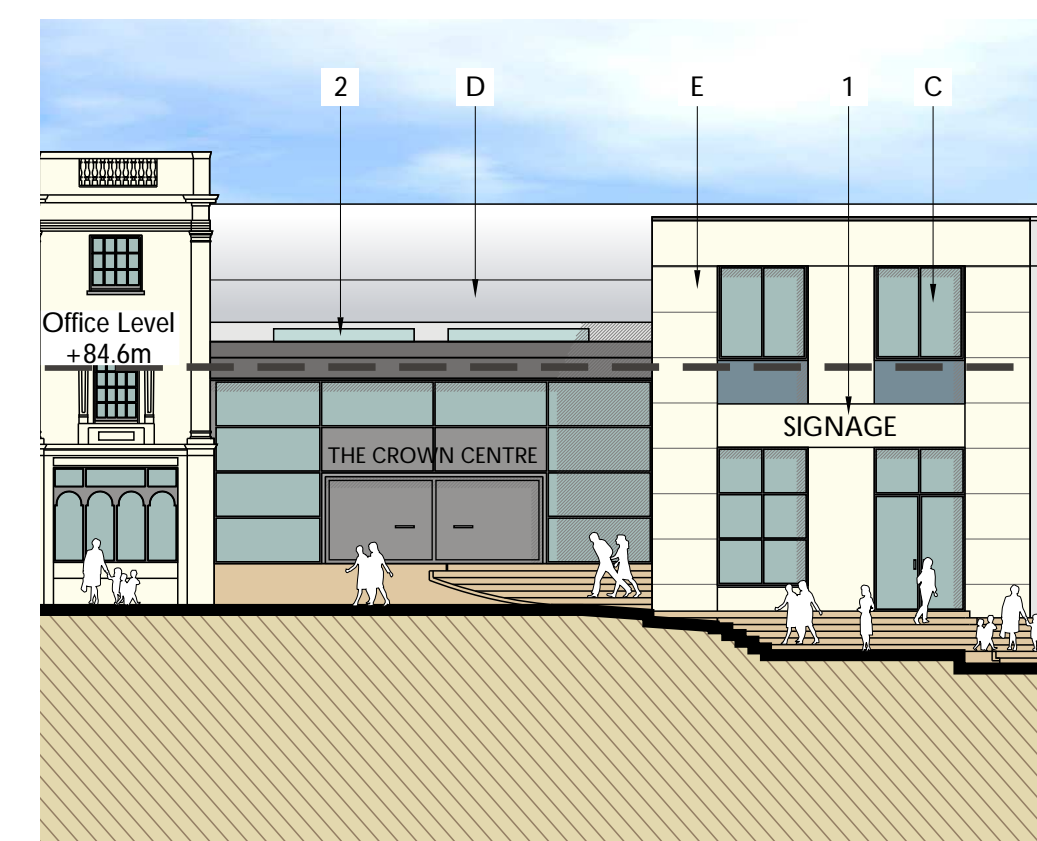
PROPOSED GROUND PLAN SCALE 1:100



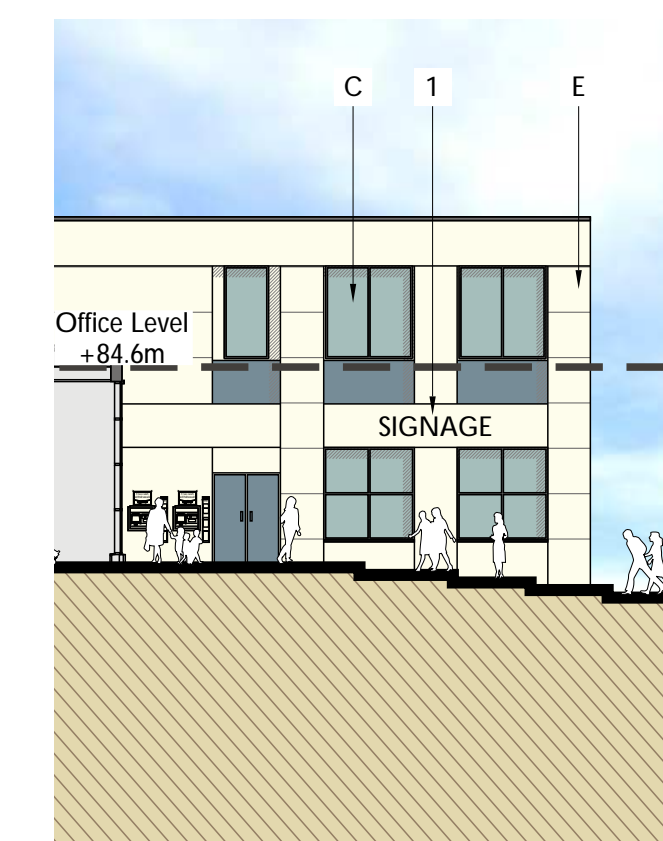
PROPOSED ELEVATION VIEW 1 SCALE 1:200



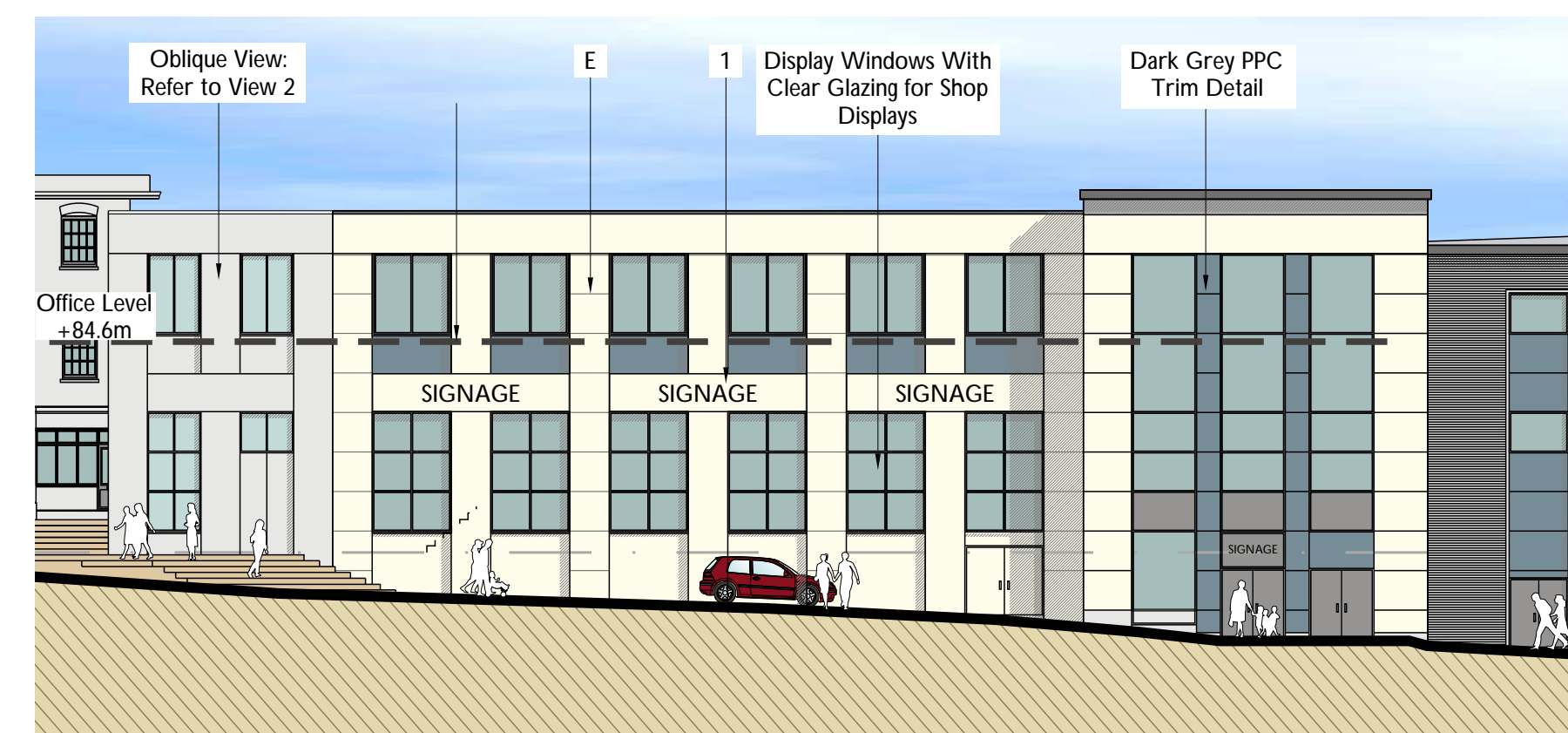
PROPOSED ELEVATION VIEW 2 SCALE 1:200



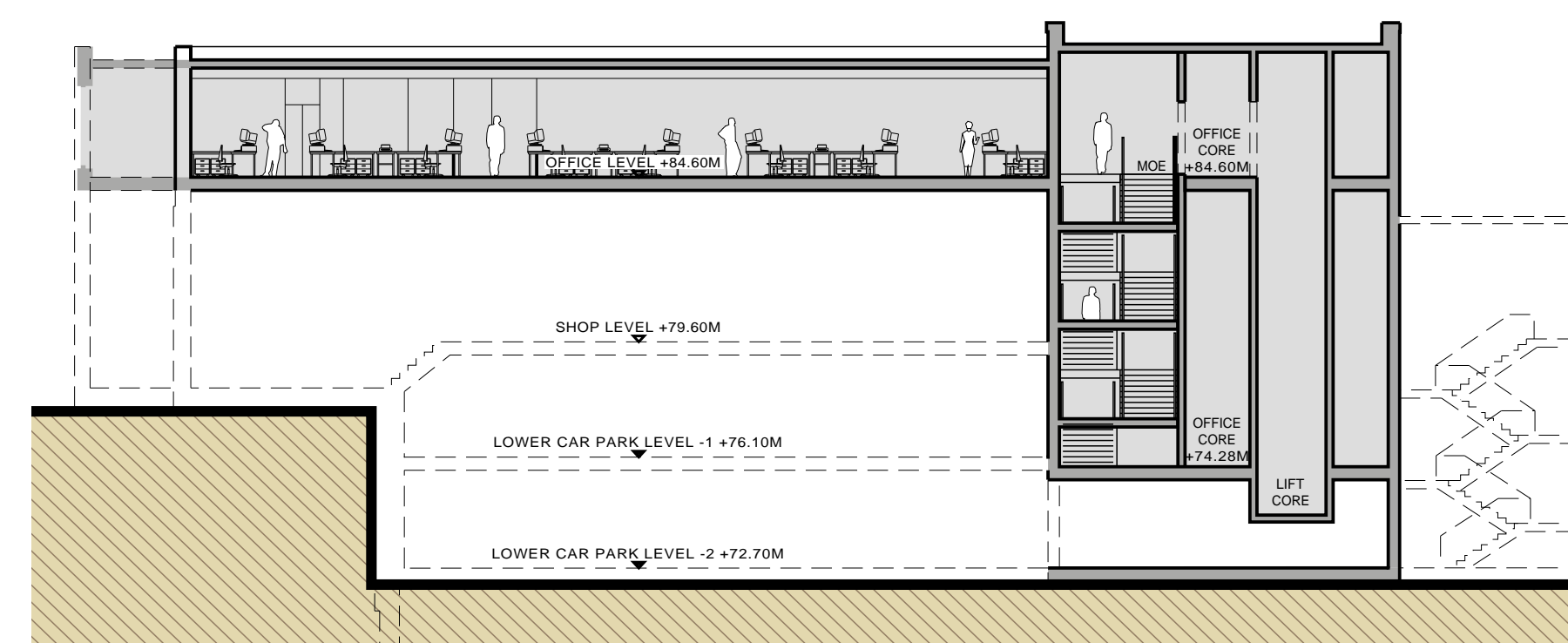
PROPOSED ELEVATION VIEW 3 SCALE 1:200



PROPOSED ELEVATION VIEW 4 SCALE 1:200



PROPOSED ELEVATION VIEW 5 SCALE 1:200

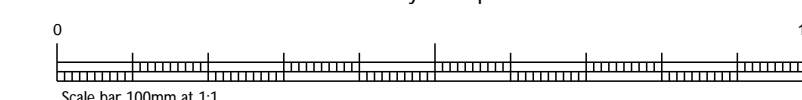


PROPOSED SECTION A - A SCALE 1:100

NOTES

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INTERNAL LAYOUT FOR ILLUSTRATIVE PURPOSES ONLY

MATERIALS

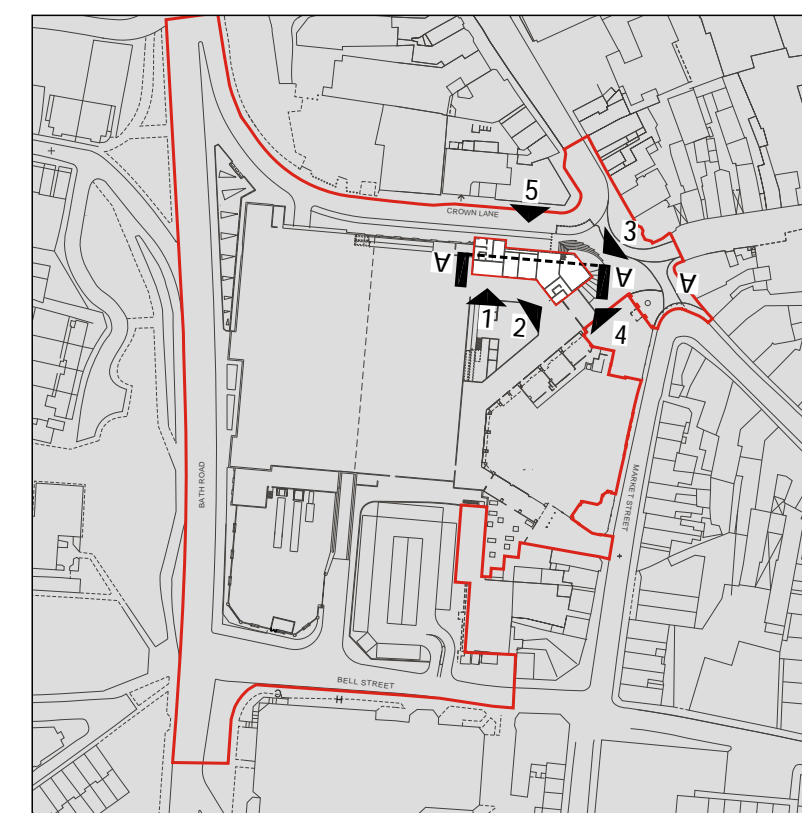
A	Timber Larch Panels
B	Composite Cladding Panel (Blue/Dark Grey)
C	Glazing
D	Kingspan KS100TD Roof Panel
E	Reconstituted Stone Panel Walling with Smooth Joints
F	Red Brickwork Wall

COMPONENTS

1	Signage shown indicative Subject to separate Application
2	Xtraflight Rooflight for Natural Daylight

LEGEND

	Site Boundary
	Existing Levels
	Proposed Levels



KEY PLAN SCALE 1:2500

REV.	DATE	NOTE	IN

Project  
**PROPOSED RETAIL DEVELOPMENT  
CROWN CENTRE  
STOURBRIDGE**

Title  
**PROPOSED OFFICE PLAN,  
SECTIONS & ELEVATIONS**

Scale  
1:100/1:200 @ A1

Date  
OCTOBER 10

Drawn  
ZM

Checked  
MW

Drawing Number  
**6376 - P120.0**

Revision  
-

Saunders Partnership Architects  
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Hertfordshire, AL7 3AX



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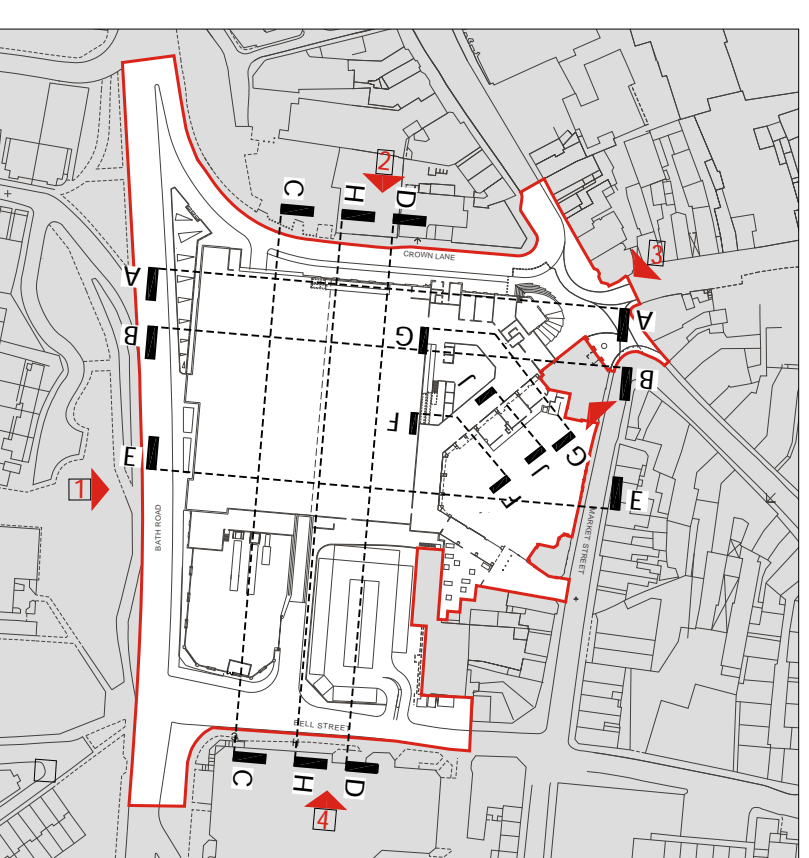


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 • Tesco Store shown is a modified F66 on site with staff on mezzanine and calls in external load count due to site constraints.  
 • Original Store footprint and figures taken from Tesco Standard Store Footprints UK concept dated 2009 02 Rev A Footprints (Sheet 2 of 4). Based on information provided by Tesco Stores Ltd.  
 • KPI figures related to the above standard footprints have been provided by Tesco Stores Ltd.  
 • Modified Stores vary from standard figures and to obtain accurate figures we would advise a Retail Layout is commissioned and agreed by Tesco.

LEGEND

	Site Boundary
--	---------------



PROPOSED ROOF PLAN

Project  
**PROPOSED RETAIL DEVELOPMENT  
 CROWN CENTRE  
 STOURBRIDGE**

Title  
**PROPOSED ROOF PLAN**

Scale  
**1:200 @A0**

Date  
**OCTOBER 10**

Drawn  
**LC**

Checked  
**MW**

Drawing Number  
**6376 - P104.1**

Revision

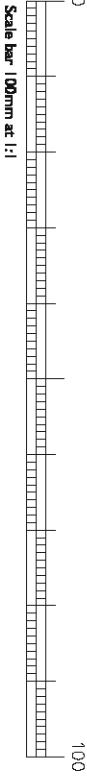
Saunders Partnership Architects  
 17 Blackwater Road  
 Weylyn Garden City  
 Herts SG13 7JX

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This drawing to be read in accordance with the specifications of Quanda and related drawings.

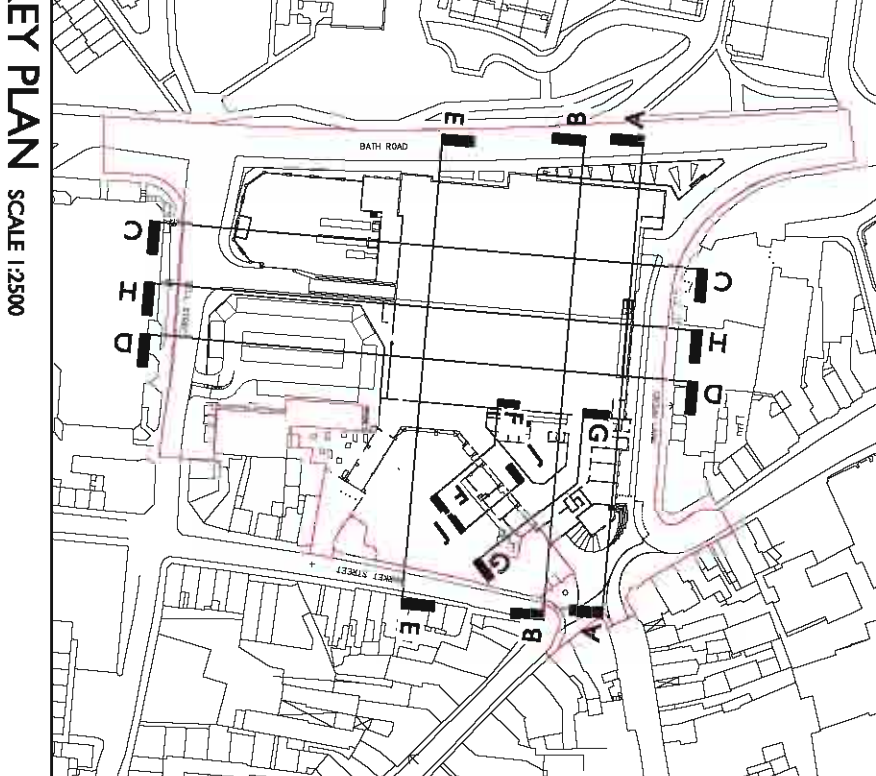
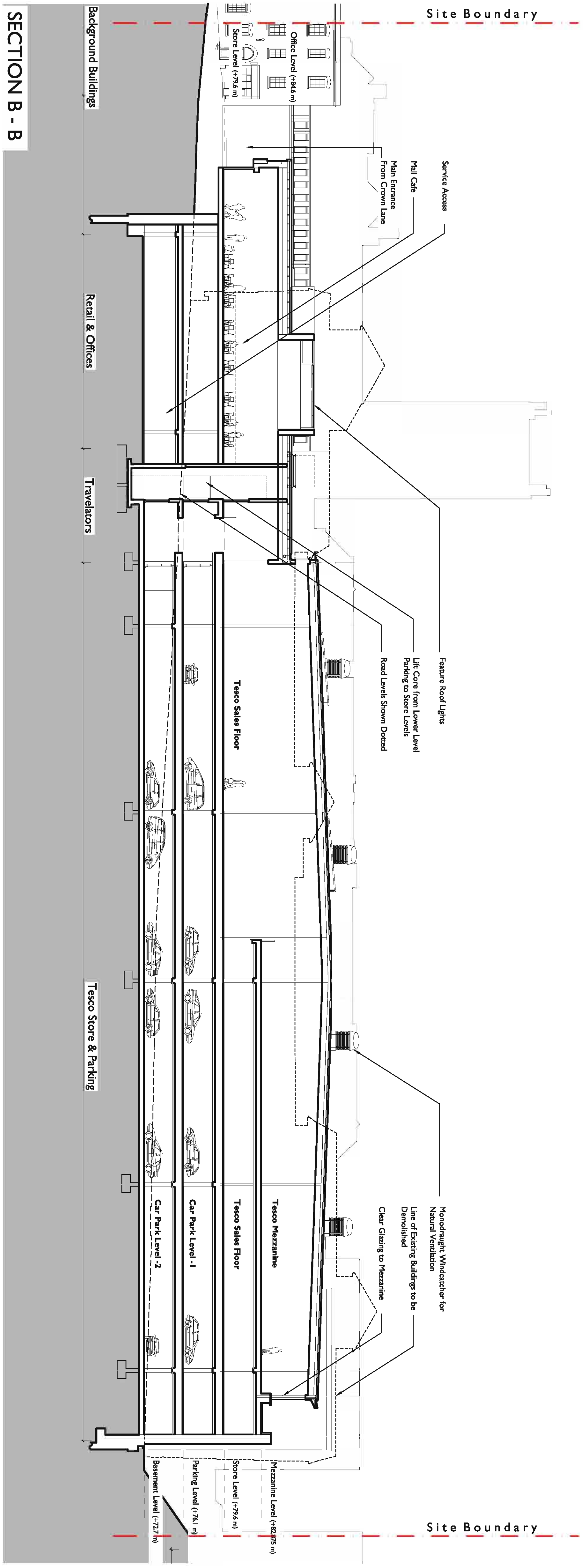
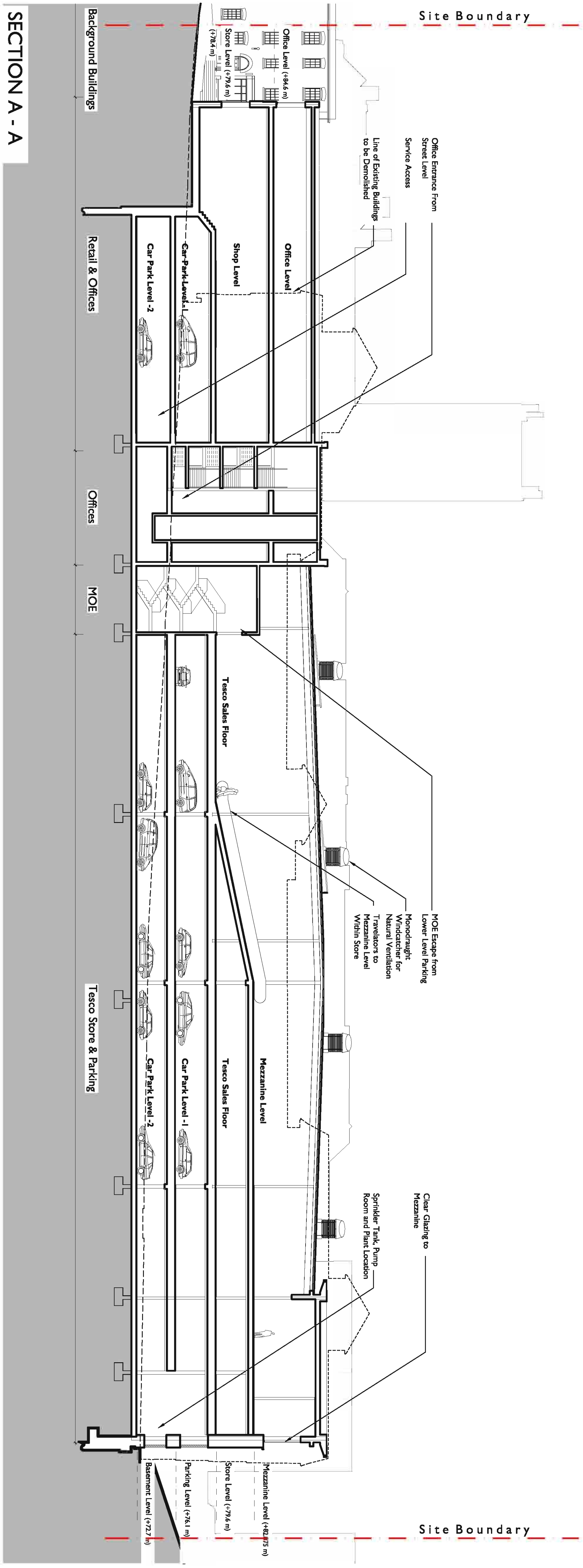
No Dimensions to be noted from this drawing. All stated dimensions to be verified on site and the Architect notified of any discrepancies.



Scale bar: 100mm at 1:1

- NOTES**
- Landscaping is indicative only and to be agreed by Landscape Architect.
  - Landscaping scheme, drainage and landscape drainage to be agreed by Landscape Architect.
  - Highways arrangement shown on our drawing is indicative only. Latest Highways design and detailed highways design is subject to agreement.
  - Existing levels have been taken from Ordnance Survey and Topographical Survey information by others.
  - Proposed levels to be confirmed by Structural and Civil Engineer and to be remediation work carried out on the site after our drawings have been issued in which instances we advise a revised Topographical Survey is carried out.

LEGEND	Site Boundary
	Site Boundary



REV.	DATE	NOTE	BY

**PROPOSED RETAIL DEVELOPMENT  
CROWN CENTRE  
STOURBRIDGE**

**PROPOSED SECTIONS**

Title	Date
Sale	1:200 @A1 SEPT 10
Drawn	Checked

Drawing Number: **6376 - P1020**

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offices at: Weymouth, Dorset City | Bristol | Manchester







**NOTES**

This drawing is to be read in accordance with the specifications/Bills of Materials and related drawings.

No Dimensions to be scaled from this drawing. All stated dimensions to be verified on site and the Architect notified of any discrepancies.

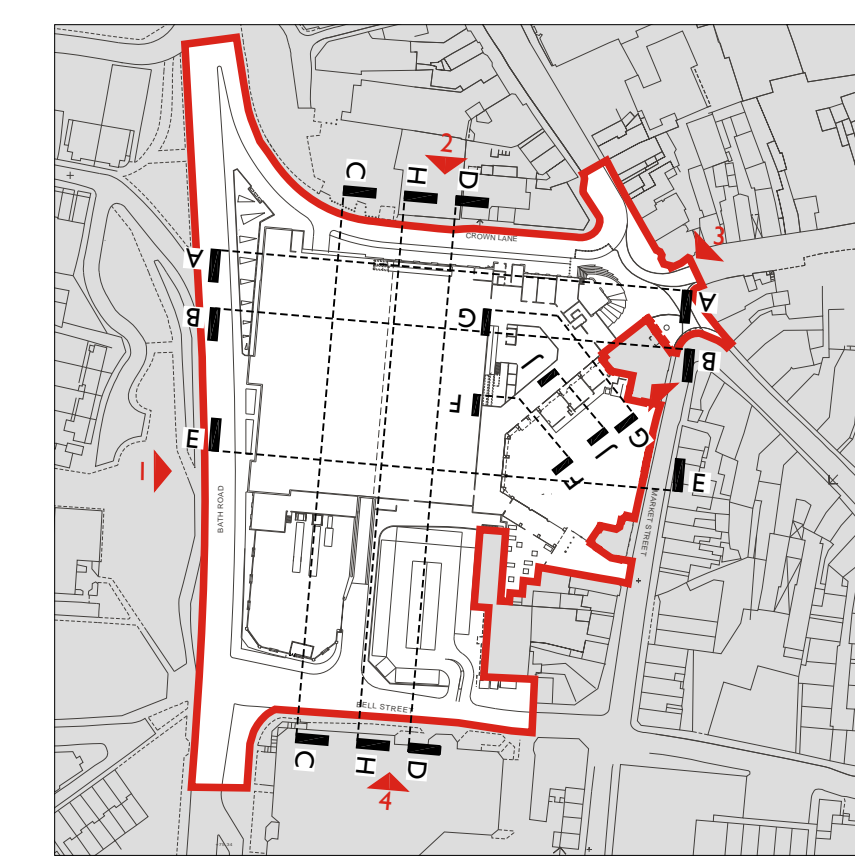
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**NOTES**

- Landscaping is indicative only and to be agreed by Landscape Architect. Refer to latest Landscape Architect drawings for accurate detailed Landscape scheme.
- Highways arrangement shown on our drawings is indicative only. Latest Highways design and detailed highways design is subject to agreement between Highways Authority and Highways Consultant.
- Existing levels have been taken from Ordnance Survey and Topographical Survey information by others.
- Proposed levels to be confirmed by Structural and Civil Engineer and to be agreed with client. Proposed levels subject to change following any remediation work carried out to the site after our drawings have been issued in which instance we advise a revised Topographical Survey is carried out.

**LEGEND**

	Site Boundary
	Proposed Retaining Wall
	Existing trees
	Proposed Trees
	Existing Levels
	Proposed Levels



KEY PLAN SCALE 1:500

REV.	DATE	NOTE

Project  
**PROPOSED RETAIL DEVELOPMENT  
 CROWN CENTRE  
 STOURBRIDGE**

Title  
**PROPOSED  
 LOWER CAR PARK PLAN  
 LEVEL -1**

Scale 1:200 @A0 Date OCTOBER 10  
 Drawn LC Checked MW  
 Drawing Number 6376 - P101.5 Revision

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PROPOSED LOWER CAR PARK PLAN LEVEL -1





EXISTING AERIAL PHOTOGRAPH N.T.S.

**NOTES**

This drawing to be read in accordance with the specification/Bills of Quantities and related drawings.

No Dimensions to be scaled from this drawing. All stated dimensions to be verified on site and the Architect notified of any discrepancies.

0 100  
Scale bar 100mm at 1:1

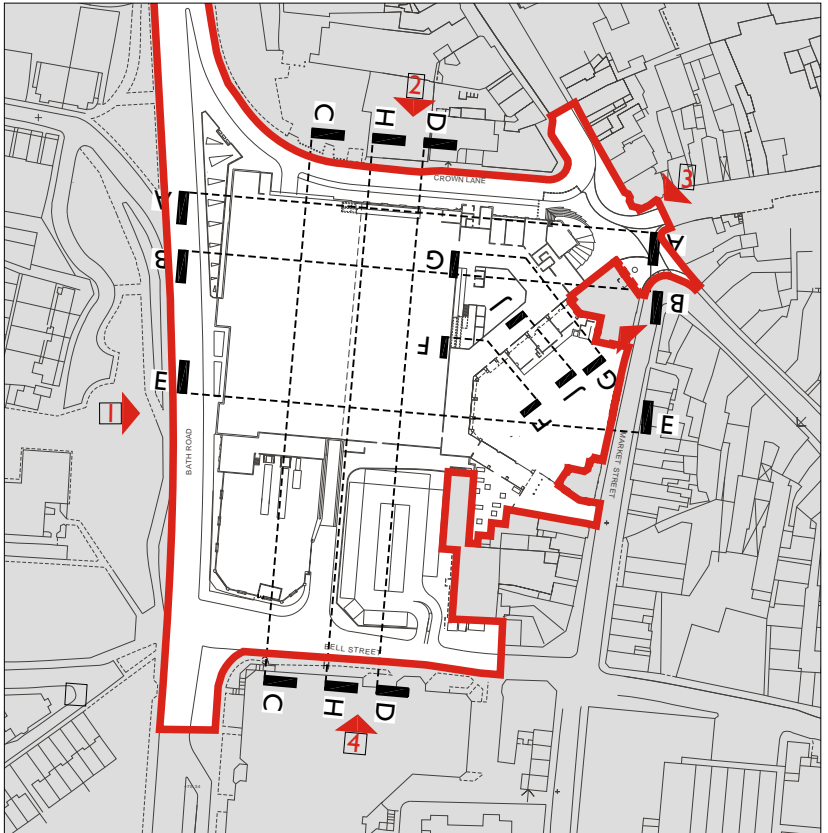
**NOTES**

- Landscaping is indicative only and to be agreed by Landscape Architect. Refer to latest Landscape Architects drawings for accurate detailed Landscape scheme
- Highways arrangement shown on our drawings is indicative only. Latest highways design and detailed highways design is subject to agreement between Highways Authority and Highways Consultant
- Existing levels have been taken from Ordnance Survey and Topographical Survey information by others
- Proposed levels to be confirmed by Structural and Civil Engineer and to be agreed with client. Proposed levels subject to change following any remediation work carried out to the site after our drawings have been issued in which instance we advise a revised Topographical Survey is carried out.

INTERNAL LAYOUT FOR ILLUSTRATIVE PURPOSES ONLY

**LEGEND**

	Site Boundary
	Existing trees removed
	Proposed Trees
+00.00M	Existing Levels
+00.00M	Proposed Levels



KEY PLAN SCALE 1:2500

REV.	DATE	NOTE	IN

Project  
**PROPOSED RETAIL DEVELOPMENT CROWN CENTRE STOURBRIDGE**

Title  
**PROPOSED MEZZANINE FLOOR PLAN LEVEL I**

Scale 1:500 @A1 Date SEP 10  
Drawn LC Checked MW

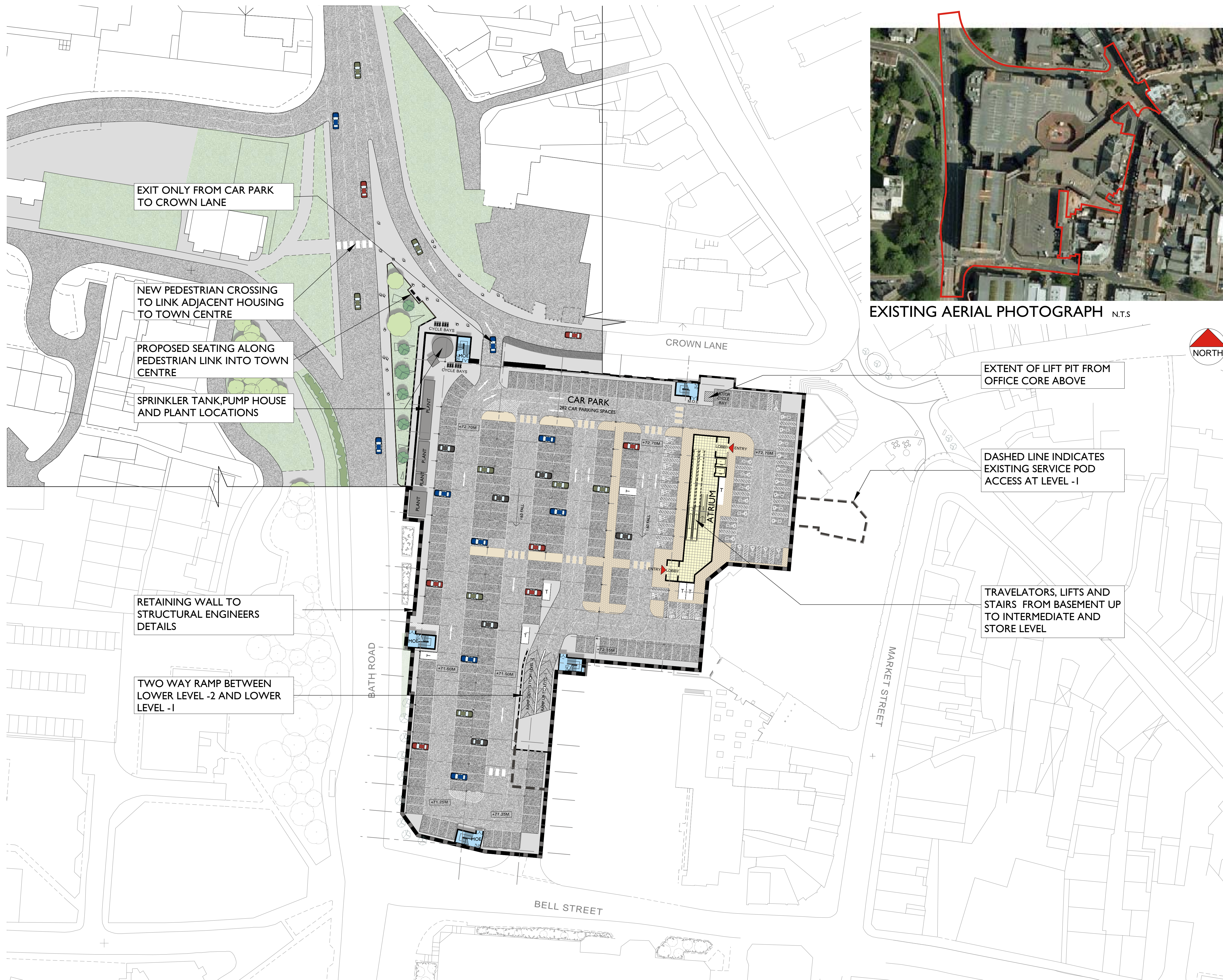
Drawing Number 6376 - P101.3 Revision

Saunders Partnership Architects  
Studio Four  
37 Broadwater Road  
Welwyn Garden City  
Hertfordshire, AL7 3AX

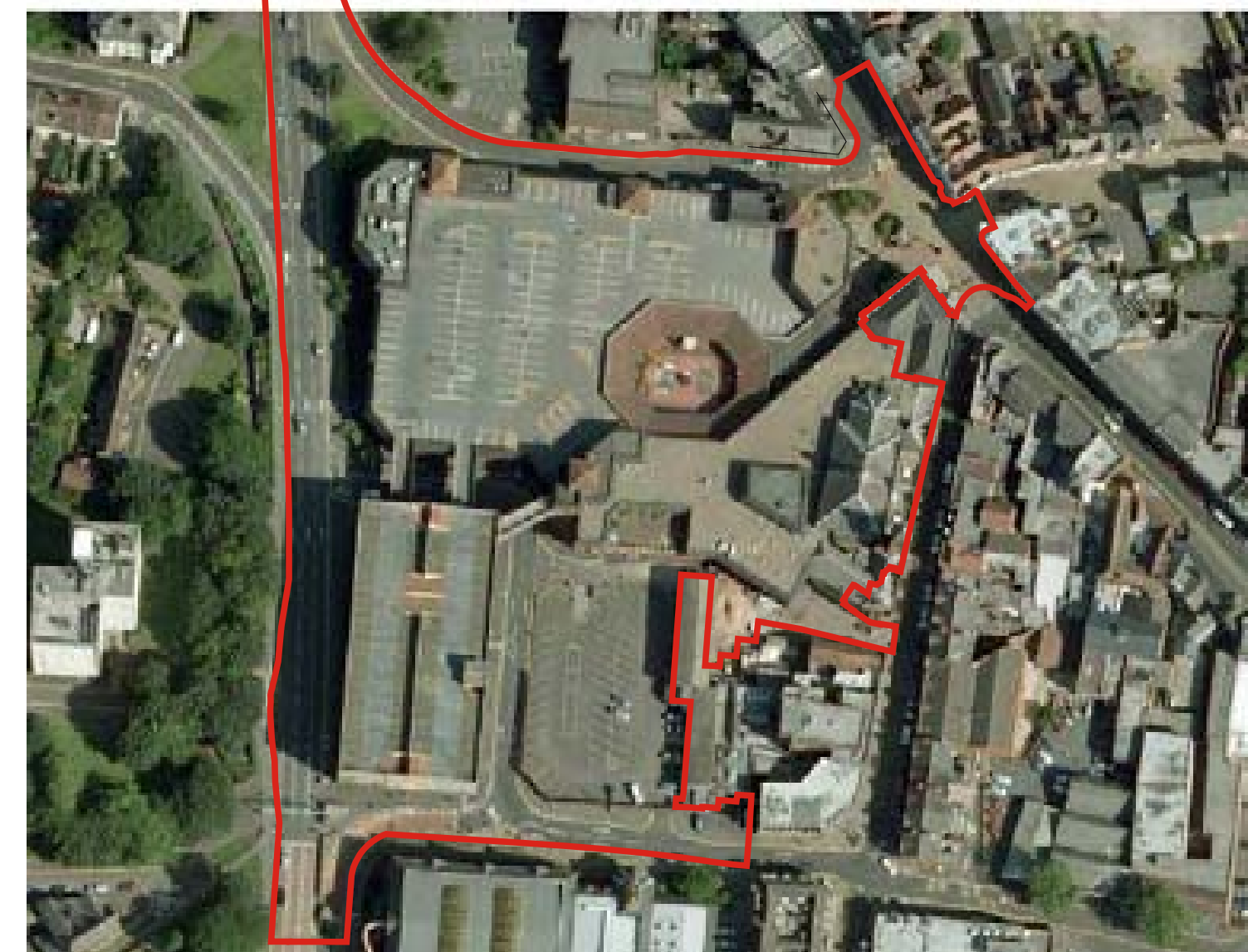
T 01707 385300  
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www.saunderspartnership.com  
offices at Welwyn Garden City | Bristol | Manchester







PROPOSED LOWER CAR PARK PLAN LEVEL -2

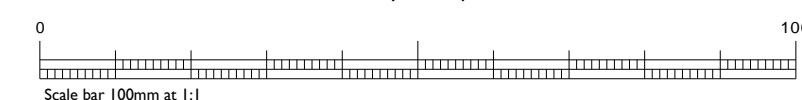


EXISTING AERIAL PHOTOGRAPH N.T.S

NOTES

This drawing to be read in accordance with the specification/Bills of Materials and related drawings.

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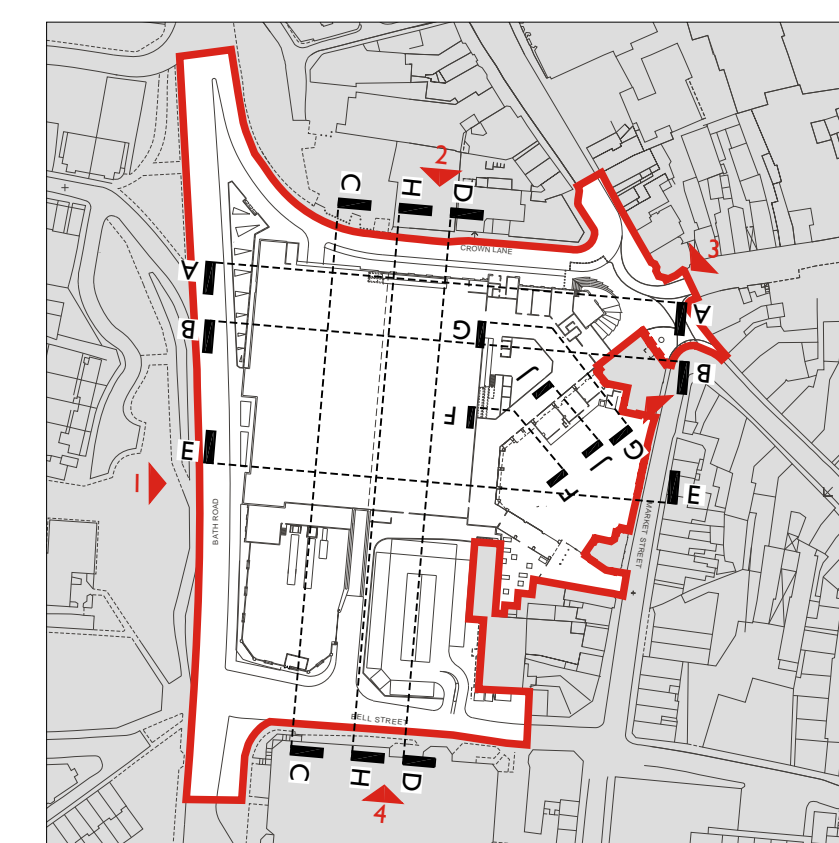


NOTES

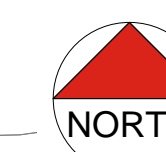
- Landscaping is indicative only and to be agreed by Landscape Architect. Refer to latest Landscape Architects drawings for accurate detailed Landscape scheme
- Highways arrangement shown on our drawings is indicative only. Latest highways design and detailed highways design is subject to agreement between Highways Authority and Highways Consultant
- Existing levels have been taken from Ordnance Survey and Topographical Survey information by others
- Proposed levels to be confirmed by Structural and Civil Engineer and to be agreed with client. Proposed levels subject to change following any remediation work carried out to the site after our drawings have been issued in which instance we advise a revised Topographical Survey is carried out.

LEGEND

	Site Boundary
	Proposed Retaining Wall
	Existing trees
	Proposed Trees
	Existing Levels
	Proposed Levels



KEY PLAN SCALE 1:2500



EXTENT OF LIFT PIT FROM OFFICE CORE ABOVE

DASHED LINE INDICATES EXISTING SERVICE POD ACCESS AT LEVEL -1

TRAVELATORS, LIFTS AND STAIRS FROM BASEMENT UP TO INTERMEDIATE AND STORE LEVEL

REV.	DATE	NOTE	IN

Project  
**PROPOSED RETAIL DEVELOPMENT  
CROWN CENTRE  
STOURBRIDGE**

Title  
**PROPOSED  
LOWER CAR PARK PLAN  
LEVEL -2**

Scale  
1:500 @A1

Date  
SEP 10

Drawn  
LC

Checked  
MW

Drawing Number  
**6376 - P101.0**

Revision

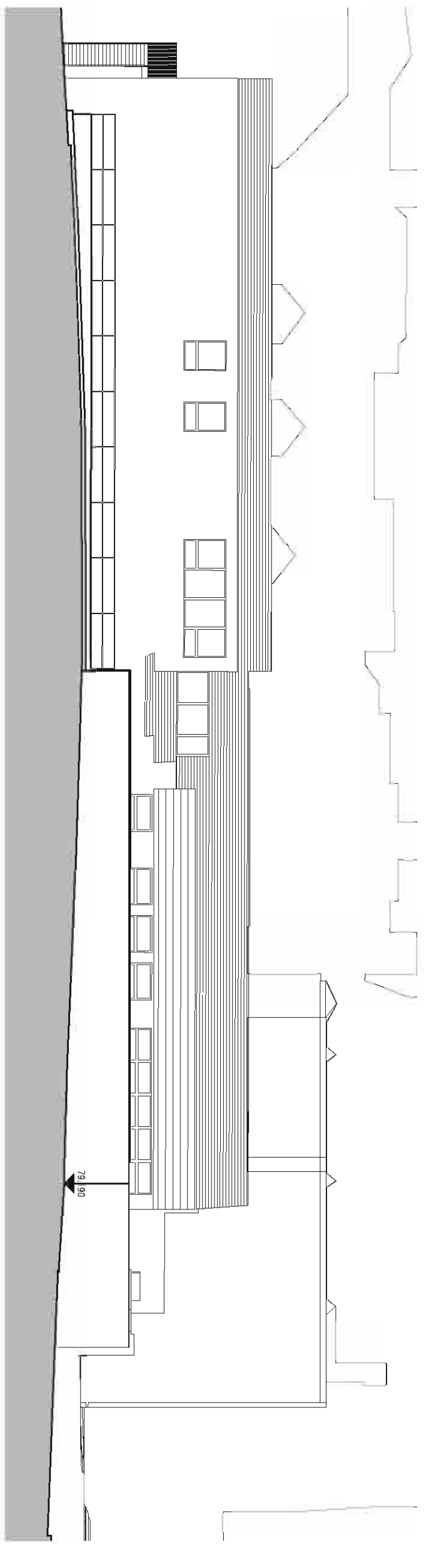
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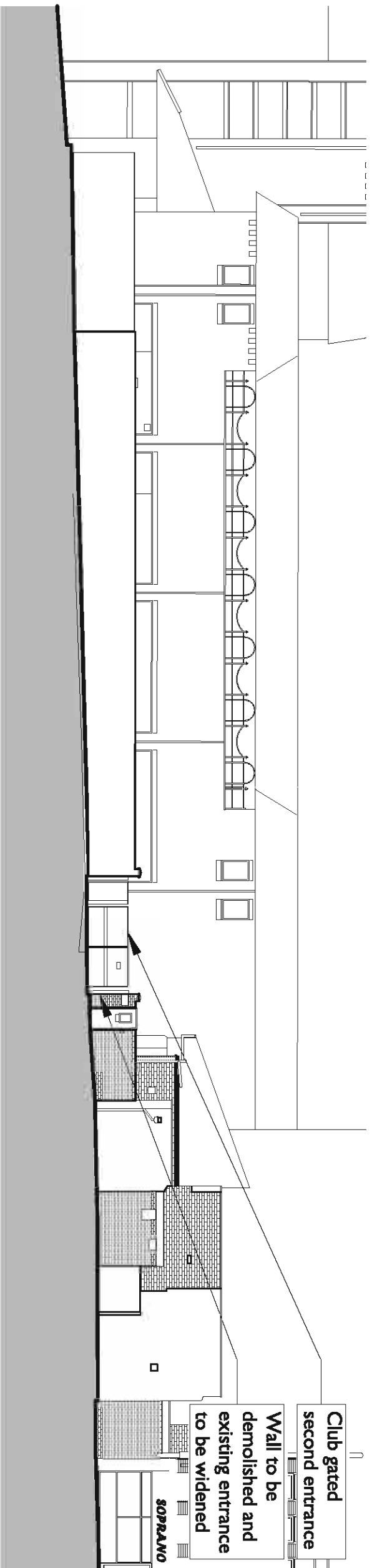
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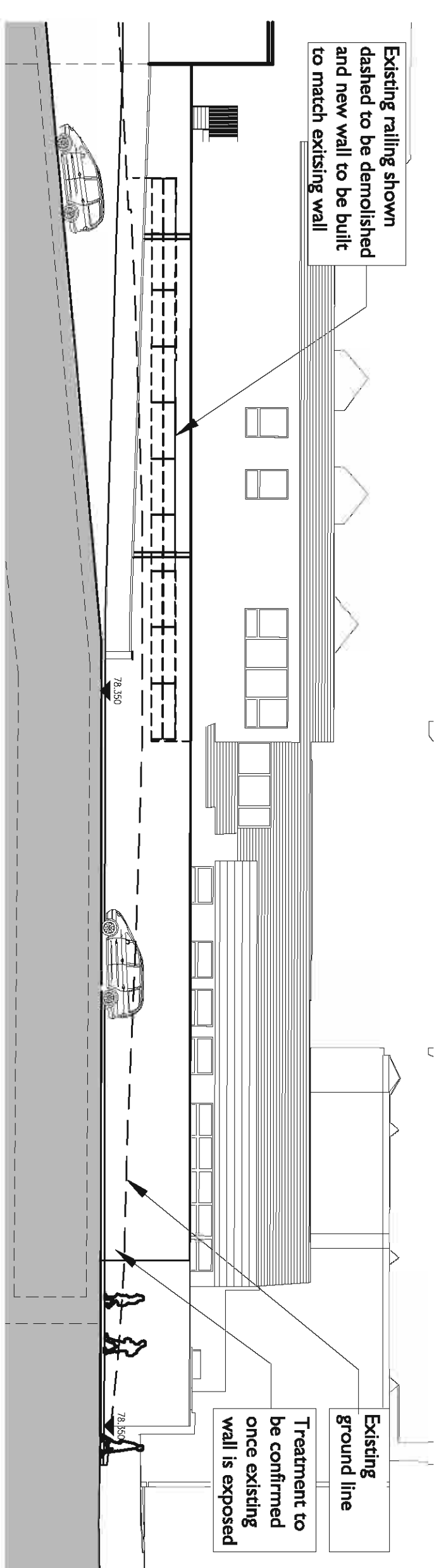




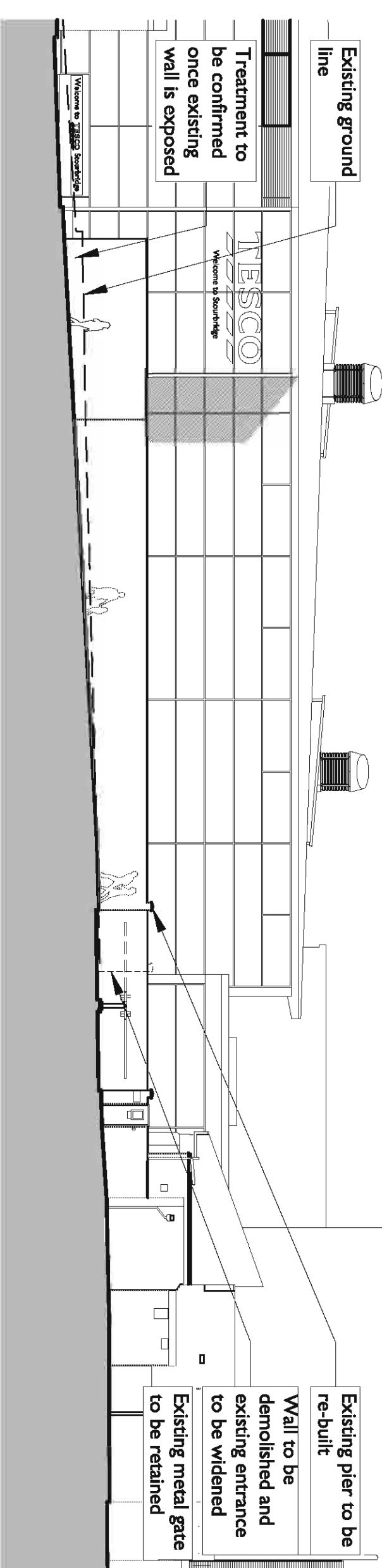
VIEW 1 EXISTING CLUB CAR PARK WALL



VIEW 1 EXISTING CLUB CAR PARK WALL



VIEW 1 PROPOSED CLUB CAR PARK WALL



VIEW 2 PROPOSED CLUB CAR PARK WALL

**NOTES**

This drawing to be read in accordance with the specifications of Quantities and related drawings.

No Dimensions to be called from this drawing. All actual dimensions to be verified on site and the Architect notified of any discrepancies.

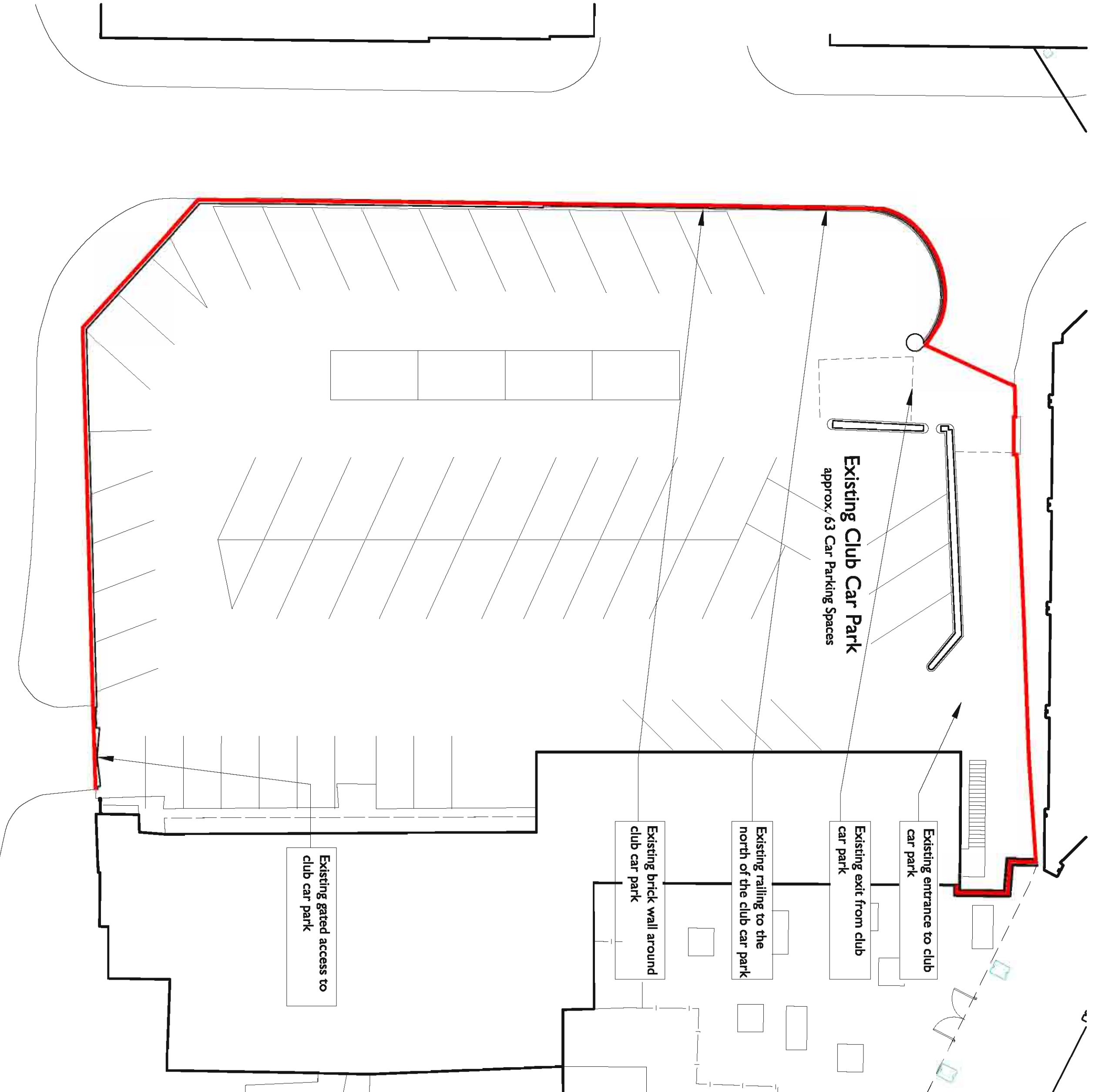
Scale bar (Down as it)

**NOTES**

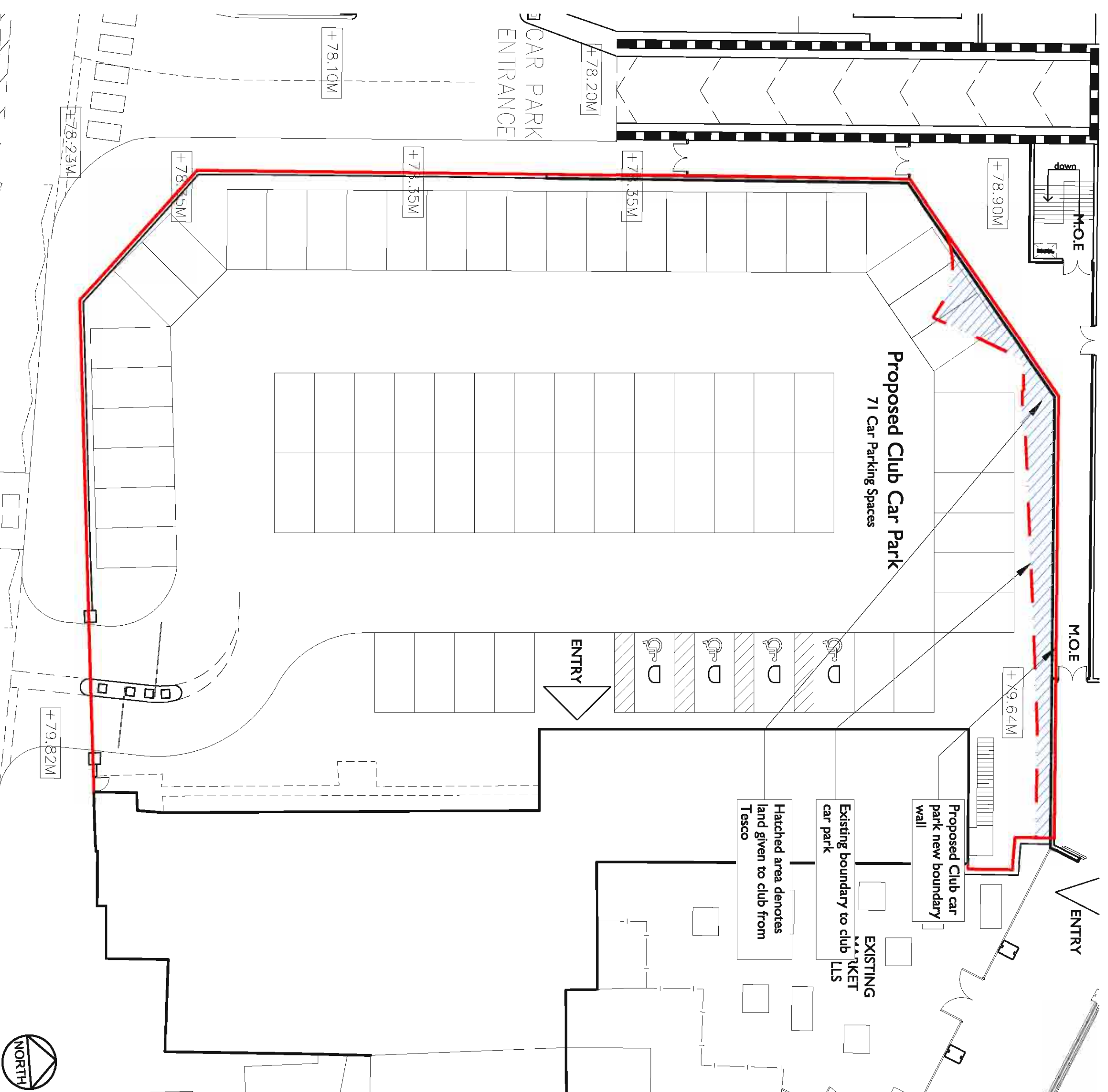
- Landscaping is indicative only and to be agreed by Landscape Architect. Landscape scheme and associated details are for guidance only.
- Highways arrangement shown on our drawings is indicative only. Latest highway design and detailed highway design is subject to agreement with the relevant highway authority.
- Existing levels have been taken from Ordnance Survey and Topographical Survey information by others.
- Proposed levels to be confirmed by Structural and Civil Engineer and to be re-confirmed on site after our drawings have been issued.
- Proposed levels to be confirmed by Structural and Civil Engineer and to be re-confirmed on site after our drawings have been issued.
- In which instances we include a revised Topographical Survey is carried out.

**LEGEND**

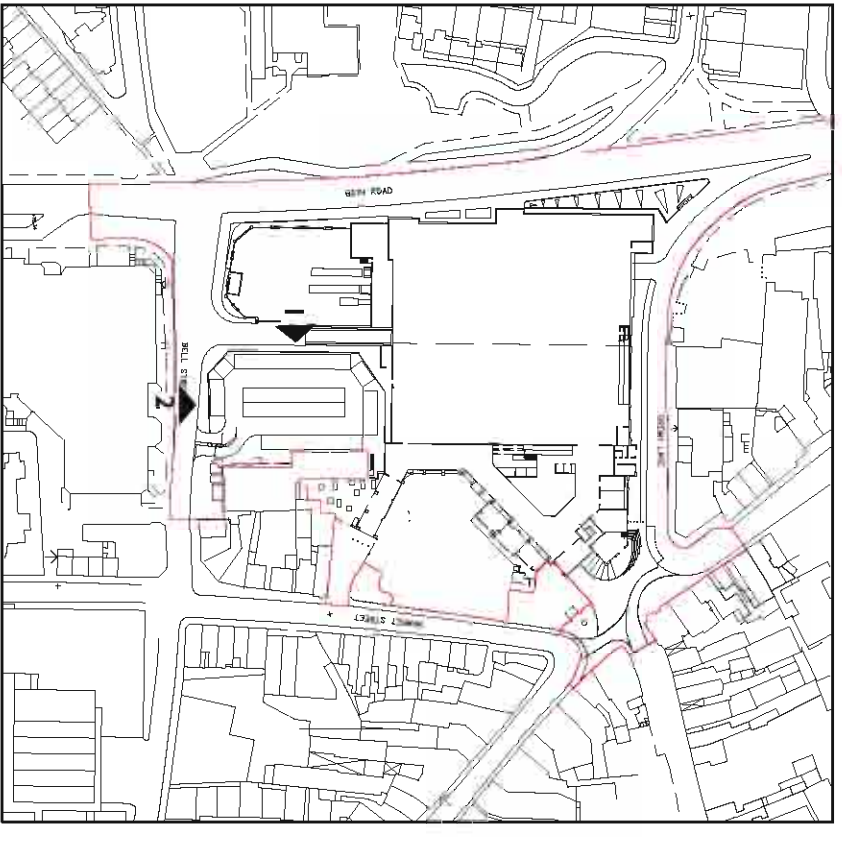
	Site Boundary
	Proposed Retaining Wall
	Existing trees
	Proposed Trees
	Existing Levels
	Proposed Levels



EXISTING CLUB CAR PARKING LAYOUT



PROPOSED CLUB CAR PARKING LAYOUT



KEY PLAN SCALE 1:2500

REV.	DATE	NOTE	BY

**PROPOSED RETAIL DEVELOPMENT  
CROWN CENTRE  
STOURBRIDGE**

**PROPOSED CLUB CAR PARK  
ALTERATION PLAN**

Scale	Date
1:200 @A1	SEPT 10
Drawn	Checked
LC	
Drawing Number	Revision
6376 - P14.1.0	

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