

PLANNING APPLICATION NUMBER:P22/1657

Type of approval sought	Full Planning Permission
Ward	Belle Vale Ward
Agent	Gould Singleton Architects
Case Officer	Catherine Golightly
Location:	HAYLEY FASTENERS (SITE 2), SHELAH ROAD, HALESOWEN, B63 3XL
Proposal	DEMOLITION OF EXISTING COMMERCIAL BUILDINGS TO FACILITATE THE ERECTION OF 1 NO. WAREHOUSE WITH ANCILLARY SUPPORT OFFICES, CAR PARK ALTERATIONS, FENCING, GATES WITH MODIFICATIONS TO ACCESS FROM PUBLIC HIGHWAY. EXTENSION AND ALTERATIONS TO EXISTING OFFICE BLOCK (3) WITH NEW FENESTRATION RAIN SCREEN CLADDING AND NEW MAIN ENTRANCE, NEW FENESTRATION RAIN SCREEN CLADDING TO EXISTING BLOCK (2). ELEVATIONAL CHANGES TO INCLUDE NEW ROLLER SHUTTER ENTRANCES. NEW UNLOADING CANOPIES BETWEEN EXISTING AND NEW WAREHOUSES AND CONTROL KIOSK TO FRONT.
Recommendation Summary:	APPROVE SUBJECT TO CONDITIONS

SITE AND SURROUNDINGS

1. The 2.9h application site comprises of an existing complex of industrial buildings, car parking, hard and soft landscaping areas that is sited within an established employment site and that is set within a wider industrial setting.
2. The site, accessed of Shelah Road, comprises of a complex of large and some smaller industrial buildings that are located largely to the

northern side of the site, with the southern boundary largely defined by car parking and soft landscaped/wooded areas. The site is currently occupied by Hayley Group whose business sector is the supply of engineering components, bearings, seals, lubricants, pumps tools and fastenings.

3. The site is located within the Coombswood to Halesowen Regeneration Corridor 14, within a designated Mixed Use Redevelopment Area (H14.3) which is to be retained for industrial purposes.
4. The site is also recognised as forming part of the Borough's area of Linear Open Space providing a Green Network linking the Hawne Colliery SINC to the west and the Stour Valley SINC to the north and east of the site.
5. The southern area of the site includes Woodland Tree Preservation Order that was confirmed in 2022.

PROPOSAL

6. This planning application seeks the demolition of 6 existing smaller buildings within the curtilage of the site, with the erection of one large replacement warehouse being erected behind the existing commercial premises fronting Shelah Road, with other associated changes being proposed to the establish parking and service areas as well as changes proposed to the façade of the main building.
7. The proposed scheme has been amended through the progression of the application which includes changes to the proposed layout of the

10. In addition to the proposed new warehouse building, modest extensions and modifications are also proposed to the existing buildings fronting Shelah Road.



11. These would include the following associated works:
- Small extensions the north elevation of Block 3, fronting Shelah Road,
 - New aluminium cladding to the front and side elevations of Block 3,
 - The creation of a new glazed primary entrance feature to the frontage of Block 3.
 - New aluminium cladding to the front elevation of Block 2,
 - New canopy feature to the west elevation of Block 2,
 - New/modified accesses into the site,

- New car parking and loading areas to be secured across the development site.
12. The proposed development would increase the number employees from an existing 279 up to 310.
13. The application has been submitted with the following supporting documents; Arboricultural Impact Assessment, Flood Risk Assessment, Drainage Impact Assessment, Energy Statement, Noise Report, Air Quality Assessment, Transport Assessment, and an Ecological Impact Assessment.

HISTORY

14. Extensive planning history for industrial/warehouse/office uses with ancillary works.

PUBLIC CONSULTATION

15. The application was advertised by way of two rounds of neighbour notification letters being sent to the occupiers of sixty three properties within close proximity, and Site and Press Notices have also been displayed. As part of the first round of consultation, 12 responses were received from 10 addresses, who raised comments on the following grounds:
- Concerns regarding the height, proximity and colour of the proposed warehouse,
 - Intrusive appearance,

- The planting of trees would not mitigate visual impacts to neighbours,
- Acoustic fence provides little protection to neighbours,
- Concerns over opening hours,
- Loss of privacy,
- Loss of light to neighbouring premises,
- Increase in traffic along Fairmile Road, increasing air pollution,
- Inadequate on site parking provision,
- Displacement of vermin into residential properties,
- Destruction of woodland,
- Loss of wildlife habitats,
- Increase in air pollution,
- Light pollution,
- Noise Pollution,
- Existing noise problems would be exacerbated,
- Subsidence concerns,
- Security concerns,
- Alternative warehouse sites available locally,
- Loss of property values.

16. In addition, three letters of support had been received from nearby addresses, offering support on the following grounds:

- Improve the appearance of the area,
- Impacts can be mitigated by tree planting and the inclusion of EVCP,
- Positive investment for businesses,
- Supports the local economy,

- The one-way traffic system will improve traffic flow on site and on adjacent roads.
17. Councillor Phipps has requested the application be heard by Planning Committee to consider matters of highway safety and ecological concerns on behalf of local residents.
18. The second round of consultation generated 6 further responses from 5 addresses. Additional comments focused on the following:
- Suggesting alternative colours for the building,
 - Impact on health,
 - Loss of existing on site parking provision,
 - Amended plans do not address the loss of trees/wildlife sufficiently,
 - Increase in HGV traffic.

OTHER CONSULTATION

19. Head of Planning and Development (Highway Engineer):
No objections subject to the inclusion of conditions to secure a Travel Plan, a revised parking layout with appropriate cycle storage being secured, the provision of Electric Vehicle Charging Points, with appropriate visibility, loading and service areas being secured.
20. Head of Planning and Development (Land Contamination Team):
No adverse comments.
21. Head of Environmental Health and Trading Standards:

No objections subject to the inclusion of conditions to restrict the hours of demolition, opening and noise associated with vehicle movements and machinery, along with the inclusion of an acoustic barrier, and the securing of a Travel Plan.

RELEVANT PLANNING POLICY

22. National Planning Guidance

- National Planning Policy Framework (2021)

Black Country Core Strategy (2011)

- CSP1 The Growth Network
- EMP1 Providing for Economic Growth
- ENV1 Nature Conservation
- ENV2 Historic Character and Local Distinctiveness
- TRAN2 Managing Transport Impacts of New Development
- ENV8 Air Quality

Dudley Borough Development Strategy (2017)

- S1 Presumption in favour of Sustainable Development
- S6 Urban Design
- S17 Access and Impact of Development in the Transport Network
- S19 Dudley Borough's Green Network
- S21 Nature Conservation Enhancement, Mitigation and Compensation
- S22 Mature Trees, Woodland and Ancient Woodland
- D2 Incompatible Land Uses
- D5 Noise Pollution

- D6 Light Pollution

Supplementary Planning Guidance/Documents

- Parking Standards SPD (2017)
- Nature Conservation SPD (2016)

ASSESSMENT

23. The main issues for consideration are:

- Principle
- Design and Layout
- Ecological Enhancements
- Neighbour Amenity
- Parking and Access
- Other Matters

Principle

24. The application site is located within a 'Regeneration Corridor' as designated by the Black Country Core Strategy Regeneration Corridor 14 (Coombswood to Halesowen) as defined by Appendix 2 of the Black Country Core Strategy. The application site is located within a wider indicatively drawn area comprising of 2.5ha of a '*mixed use redevelopment area*'. The recognised opportunities of the area focus on the enhancement of the establish employment base by '*improving the quality of the existing industrial building stock*'. The Dudley Borough Development Strategy goes further to suggest that this employment area should be utilised for the '*retention of industrial areas except for*

some peripheral housing development'. This Regeneration Corridor is also recognised for playing a 'significant part of the biodiversity network, supporting and encouraging biodiversity and the penetration of habitats into the sub-region from the surrounding green belt areas and the Leasowes Historic Park.'

25. The proposed development would utilise and repurpose land and buildings to support the function of the established business, to provide additional warehousing provision to serve the existing business. Given that the proposed use of the structure would be compatible with the existing uses within the employment area and it would provide additional employment opportunities, the proposal would support economic growth within this employment land corridor, thereby being in accordance with Policies CSP1 and EMP1 of the Black Country Core Strategy and the objectives of the Regeneration Corridor 14 as identified within the Dudley Borough Development Strategy. It is therefore considered that the principle of this development is acceptable in this location.

Design and Layout

26. The proposed warehouse building would be functional in design built of profiled steel cladding which is appropriate within this industrial setting and would match the colour and appearance of the existing buildings within the locality to give a coherent and uniformed appearance. Whilst it is large in scale, this has been mitigated by being sited at a lower ground level to that of the existing ground to ensure the development is

not overbearing as viewed from the rear, particularly in respect of nearby residential properties.

27. The proposed building would also be of a smaller footprint than the existing Block 3 and would be of a scale that reflects the built form locally. It would be seen in context with other larger industrial sized buildings that surround the application site. As such, the proposed unit, subject to it being sited at the suggested ground level, would not form an incongruous or overly prominent feature within this location and would have limited impact on the wider view of the site.
28. Notwithstanding objections that have been raised by neighbouring residents in terms of the height of the proposed building and overbearing impact on neighbouring amenities, resulting in the loss of light and privacy to the adjoining properties, the distance between the warehouse and neighbouring properties would be in excess of 29m at its nearest point. This would exceed the distance separation standards as outlined within the New Housing Development SPD, which seeks to secure a minimum of 14m between a dwelling and an opposing gabled wall, and requires 22m between building faces. Whilst it is noted that the development is not seeking to secure a new dwelling, the height of the structure at approximately 8.25m above ground level is not dissimilar to that of a dwelling as viewed from the rear, therefore, the impacts on the neighbours would not be dissimilar. Furthermore, its impact will be mitigated by the presence of the existing landscaping and additional mitigation tree planting that is proposed to the rear of the warehouse, which would, as currently exists, largely screen the development from view from the neighbouring dwellings.

29. The proposed modifications to the existing Blocks 2 and 3 would provide modest alterations and a comprehensive upgrade to the existing premises to improve their appearance within the street. The proposed cladding would replace the existing brick façade of the host buildings. However, the replacement materials and design modifications would not be out of keeping within this industrial setting, the nature of which would improve and enhance the appearance of the established building within the wider industrial estate. Therefore, the nature of these proposed works are acceptable.
30. In consideration of the above, the principle of the new development would not result in an addition that would appear out of character and appearance within the existing industrial setting. Furthermore, the proposed development, along with the upgrade of the existing buildings would not adversely impact the amenities, including outlook, of the neighbouring occupiers. In this respect there are insufficient grounds to warrant a refusal of the application. Therefore, and in consideration of the design and mitigation that has been incorporated as part of the proposal, the development would comply with Policy ENV2 of the Black Country Core Strategy and policies S1 and S6 of the Dudley Borough Development Strategy.

Ecological Enhancements:

31. As noted earlier in this report, the application site falls within part of the Borough's Green Network, providing a green wedge between conflicting land uses and forms a green space corridor linking the designated

Hawne Colliery SINC with the Stour Valley SINC to the north and east of the site. As such, significant consideration needs to be given to the environmental impacts of development in this location.

32. Policy ENV1 – Nature Conservation of the Black Country Core Strategy seeks to safeguard nature conservation within the area ensuring that, amongst other matters, that *“important habitats and geological features are protected from development proposals which could negatively impact upon them”*; seeking to protect the *“movement of wildlife within the Black Country and its adjoining areas, through both linear habitats (e.g. wildlife corridors) and the wider urban matrix (e.g. stepping stone sites) is not impeded by development”*. In consideration of the above, and in acknowledging where the strategic benefits of the development outweigh the importance of nature conservation sites, the policy stipulates that any such impact must be mitigated and compensated. Such measures could include the expansion of nature conservation sites, improving wildlife movements and by restoring or creating new habitats.
33. In support of Policy ENV1, Policy S21 - Nature Conservation Enhancement, Mitigation and Compensation of the Dudley Borough Development Strategy outlines the Council will *“safeguard and enhance designated nature conservation sites, habitats and features through the development process and in accordance with the Core Strategy, in particular Policy ENV1”*. In doing so, the policy outlines that where *“development clearly outweigh the nature conservation importance of the area impacted upon, Dudley Council will ensure any damage or loss of nature conservation assets is fully offset by additional nature*

conservation improvement works. It will be expected that these will normally be accommodated on-site”.

34. In consideration of the above, the application has been submitted with an Ecological Impact Assessment which concluded that no protected species would be adversely affected as a result of the proposal, and this is not disputed. Despite this, the submitted assessment encourages the introduction of a suite of on-site ecological enhancement measures that are appropriate and necessary to off-set and improve the ecological merits of the site and this has been conditioned accordingly. This includes the introduction of bird and bat boxes, and invertebrate boxes to support local wildlife.
35. Although, the proposed development would result in the loss of part of the established green wedge, the scheme has been amended following Officer advice, including that of the Council’s Tree Officer, to vary the rear car parking area. This has been done, not only to ensure the protection of established protected trees, but also to allow for an improved replacement tree planting/landscaping scheme to compensate and improve the ecological merits of the development. These changes allow for appropriate mitigation and enhancement measures to be secured, including the retention of 20 of the existing trees, incorporating the planting of an additional 22 trees, increasing the width of an enhanced and retained woodland buffer. Not only would this allow for the integrity of the woodland corridor to be retained it also helps ensure the improved level of biodiversity within this landscape buffer is supported. These protection and enhancement measures have been conditioned accordingly as part of this approval.

36. As such, and despite some encroachment within the existing landscape buffer, the proposed nature enhancement measures being incorporated would offset the environmental impacts of the proposed development. This ensures compliance with Policies ENV1 of the Black Country Core Strategy and Policies S19, S21 and S22 of the Dudley Borough Development Strategy.

Neighbour Amenity

37. The proposed development is located within an area of industrial uses and the proposed use would be consistent with the wider uses of the industrial estate. Whilst objections have been raised by neighbouring occupiers in terms of adverse light, noise and disturbance, modifications have been secured through the progression of the application to ensure appropriate protection and mitigation measures are in place. Such mitigation includes the inclusion of an acoustic fence and conditions to restrict hours of operation and demolition, to protect the nearby residents from any further impacts of the proposed development. These protection measures are considered sufficient to protect the nearby residents and therefore the application has been conditioned accordingly. These have been recommended by the Head of Environmental Health and Trading Standards who raised no objection to the proposed development subject to the recommended conditions which will be attached to the decision. Thereby, the proposal complies with policies D2, D5 and D6 of the Dudley Borough Developments Strategy.

38. Further comments have also been raised regarding possible security issues to nearby residents, however, the rear gardens of the adjacent properties are sited to the extreme rear boundary of the site, would be centrally located away from the public realm and these would not be any further exposed than that of the existing situation. Therefore, the proposal would not result in an increased security risk to nearby neighbours.

Parking and Access

39. As part of the proposed development, the existing access arrangements and parking associated within the application site is to be modified, although the primary route through the existing industrial estate will remain unaltered. Neighbouring objections have been raised on highway safety grounds as a result of an increase in traffic and the increase parking requirements associated with the proposal. However, having regard to the maximum parking standards required for such uses, the revised parking layout along with the additional information provided as part of the submission, demonstrates that the revised parking layout would accommodate for the increased parking demand associated with the development, reducing the need for on-street parking locally. For clarity, there are 165 staff and visitor parking spaces as existing and 220 proposed.
40. This has been reviewed by the Council's Highway Engineer who considers that, subject to appropriate conditions being attached, the highway impacts of the development can be sufficiently mitigated for. These recommended conditions have been attached accordingly.

41. Therefore, and subject to these suggested conditions, the revised access arrangements and parking provision associated with the proposed development would not result in significant adverse highway safety concerns in accordance with Policy TRAN2 of the Black Country Core Strategy and Policy S17 of the Dudley Borough Development Strategy.
42. Notwithstanding this, and having regard to the recommendations contained within the Parking Standards SPD and as suggested by the Highway Engineer, there is a requirement for appropriate Electric Vehicle Charging Points (EVCP) to be provided to promote the use of ultra low emissions vehicles. This provision has been sought by way of a condition, and as such, the proposal would be in accordance with the policy requirements contained within the Policies ENV8 and DEL1 of the Black Country Core Strategy and the requirements contained within the Parking Standards SPD.

Other Matters:

43. Comments received as part of the consultation process have highlighted a number of non-material planning considerations, for example, the displacement of vermin, land stability and the effect on property prices. However, these matters do not form a material consideration in the assessment of the application and these matters cannot be taken into consideration as part of this decision making process.

44. Whilst matters relating to land stability have been raised, this is a matter for building regulations and not a planning consideration in this instance.

CONCLUSION

45. The proposed development would support economic growth within an employment land corridor and will assist with job creation within the Borough. The proposed development, through careful and conditioned enhancements, will mitigate against the ecological impacts of development within this sensitive location. The proposed building and the additional modifications would be of an appropriate scale and design within the context of the existing industrial area and the siting of the building would not have an adverse impact upon residential amenities or the wider visual amenity of the area. The access arrangements and proposed parking provision on site would not adversely impact highway safety in this locality and would, through the increased level of parking reduce the level of off-site parking.
46. As a result of the above, the proposed development complies with both local and national policy and, in accordance with the NPPF should be supported.

RECOMMENDATION

47. It is recommended that the application be APPROVED subject to conditions.

Conditions and/or reasons:

1. The development hereby permitted shall be begun before the expiration of three years from the date of this permission.
REASON: To comply with Section 91(1) of the Town and Country Planning Act, 1990 as amended by Section 51 of the Planning and Compulsory Purchase Act 2004.
2. The development hereby permitted shall be carried out in accordance with the following plans and amended plans: As submitted on the 23/11/2023:

21-1883-001A
21-1883-002A
21-1883-003A
21-1883-004A
21-1883-005A
21-1883-006A
21-1883-007A
21-1883-008A
21-1883-010A
21-1883-013A
21-1883-014A
21-1883-016A
21-1883-019A
21-1883-020A
21-1883-021A

As submitted on the 03/03/2023:
Landscape Plan Drawing No.1B

As submitted on the 21/03/2023:
11884-E-001 Rev PL2

As submitted on the 05/05/2023:
21-1883-011C
21-1883-012B
21-1883-015B
21-1883-017B

21-1883-018B

21-1883-024A

21-1883-025A

21-1883-026A

REASON: For the avoidance of doubt and in the interests of proper planning.

3. No above ground development shall commence until a schedule of the types, colours and textures of the materials to be used on the external surfaces of the existing and proposed buildings hereby approved have been submitted to and approved in writing by the Local Planning Authority. The development shall thereafter be carried out in complete accordance with the approved details unless otherwise agreed in writing by the Local Planning Authority.

REASON: In the interests of the visual amenities of the area and to comply with BCCS Policies CSP4 and ENV2 and Borough Development Strategy 2017 Policy S6 and Policy D2 (in part).

4. No Development (excluding demolition works) shall take place until details of an Engineer designed no dig hard surface construction for the proposed new car parking area within the Root Protection Area (RPA) of the retained trees has been submitted to the Local Planning Authority and approved. These details shall also include the proposed geo cell construction and details of the materials for the final wearing surface.

REASON: To ensure the continued wellbeing of trees in the interests of the amenity of the area and to accord with Section 7.4 of BS 5837:2012 Trees in Relation to design, demolition and construction.

5. Before any operations being undertaken on site,
 - (a) a scheme for the protection of the retained trees shall be produced in accordance with BS5837:2012 (Trees in Relation to Design, Demolition and Construction - Recommendations), which provides for the retention and protection of trees, shrubs and hedges growing on or adjacent to the site, including trees which are the subject of the Tree Preservation Order currently in force, shall be submitted to and approved in writing by the Local Planning Authority. No development or other operations shall take place except in complete accordance with the approved protection scheme.
 - (b) No operations shall be undertaken on site in connection with the development hereby approved (including any tree felling, tree pruning, demolition works, soil moving, temporary access construction and / or widening or any operations involving the use of motorised vehicles or construction machinery) until the protection works required by the approved protection scheme are in place.

(c) No excavations for services, storage of materials or machinery, parking of vehicles, deposit or excavation of soil or rubble, lighting of fires or disposal of liquids shall take place within any area designated as being fenced off or otherwise protected in the approved protection scheme.

(d) Protective fencing shall be retained intact for the full duration of the construction of the development hereby approved and this shall not be removed or repositioned without the prior written approval of the Local Planning Authority.

REASON: To ensure the continued wellbeing of the trees in the interests of the amenity of the locality having regard to Policy S22 of the Dudley Borough Development Strategy (2017).

6. The existing trees shown on the approved plans to be retained shall not be damaged or destroyed, uprooted, felled, lopped or topped during the construction period of the development without prior written consent of the local planning authority. Any trees removed without such consent or dying or being seriously damaged or diseased during that period shall be replaced with healthy trees of such size and species as may be agreed in writing by the Local Planning Authority.

REASON: To maintain the visual and environmental quality of the site and surrounding area in accordance with the Borough Development Strategy (2017) Policy S22 (in part).

7. The development shall not be occupied until a Travel Plan has been submitted to and approved in writing by the local planning authority, including details of a Travel Plan Co-ordinator, Staff Travel Survey, Car Parking Management, Public Transport, Walking and Cycling initiatives, Publicity and Marketing, Set targets and monitoring and to join Company Travel Wise in Dudley together with a timetable for the implementation of each such element. The Travel Plan shall be implemented in accordance with the details approved by the Local Planning Authority and remain operational for the life of the development.

REASON: In the interests of highway safety and to comply with Borough Development Strategy 2017 Policy S6, Policy L1, Policy D2 (in part) and policies CSP5, DEL1, TRAN2, CEN8 and TRAN5.

8. The development hereby permitted shall not be occupied until the details of the revised parking layout have been submitted to and approved in writing by the Local Planning Authority. The parking areas shall thereafter be laid, surfaced and drained in accordance with the details shown on the approved plans prior to the development being first used and these shall thereafter be retained

for the life of the development.

REASON: In the interests of highway safety and to comply with Borough Development Strategy 2017 Policy S6, Policy D2 (in part) and policies CSP5, DEL1, TRAN2, CEN8 and TRAN5.

9. No above ground development shall commence until details of electric vehicle charging bays with a vehicle charging point, to be provided in accordance with the Council's standard (Parking Standards SPD) have been submitted and approved in writing by the Local Planning Authority. Such details shall include signs and bay markings indicating that bays will be used for parking of electric vehicles only whilst being charged. Prior to first occupation, the electric charging points and bays shall be installed in accordance with the approved details and shall thereafter be maintained for the life of the development.

REASON: In the interests of creating a sustainable form of development and to encourage the use of ultra low emission vehicles in accordance with Policies ENV8 and DEL1 of the Black Country Core Strategy.

10. The development shall not be occupied until details of secure and covered staff cycle storage and shower facilities have been submitted to and approved in writing by the Local Planning Authority. These facilities shall be provided in accordance with the approved details prior to the first use/occupation of the development and shall thereafter be retained and maintained for no other purpose for the life of the development.

REASON: In the interests of highway safety and to comply with Borough Development Strategy 2017 Policy S6, Policy D2 (in part) and policies CSP5, DEL1, TRAN2, CEN8 and TRAN5.

11. The development shall not be occupied until details of secure powered two wheeler parking have been submitted to and approved in writing by the local planning authority. These facilities shall be provided in accordance with the approved details prior to the first occupation of the development and shall thereafter be retained and maintained for no other purpose for the life of the development.

REASON: In the interests of highway safety and to comply with Borough Development Strategy 2017 Policy S6, Policy L1, Policy D2 (in part) and policies CSP5, DEL1, TRAN2, CEN8 and TRAN5.

12. The development shall not be occupied until the loading and service vehicle access areas have been laid out in accordance with the approved details and shall thereafter be maintained for the life of the development.

REASON: In the interests of highway safety and to comply with

Borough Development Strategy 2017 Policy S6, Policy D2 (in part) and policies CSP5, DEL1, TRAN2, CEN8 and TRAN5.

13. No part of the development shall be occupied until visibility splays to the new access have been provided at the junction between the proposed means of access and the highway with an 'x' set back distance of 2.4 metres and a 'y' distance of 59 metres. No structure or vegetation exceeding 600mm in height above the adjoining highway shall be placed or allowed to grow within the visibility splay for the life of the development unless otherwise agreed in writing by the Local Planning Authority.

REASON: In the interests of highway safety and to comply with Borough Development Strategy 2017 Policy S6, Policy D2 (in part) and policies CSP5, DEL1, TRAN2, CEN8 and TRAN5.

14. The proposed development will only be operational between the hours of 06.00 to 22.00 Monday to Friday; 06.00 to 14.00 on Saturday and not at all on Sunday or Bank Holidays. Such operations include loading, unloading and general warehouse activities, including deliveries and forklift truck movements to the warehouse and associated haulage yard. This condition will not apply to loaded vehicles leaving the site between 22.00 to 06.00 hours Monday to Saturday. Vehicles leaving the site between 22.00 to 06.00 hours Monday to Saturday will not carry out any reversing manoeuvres and will only use forward gear to depart the site.

REASON: To protect the amenities of nearby residents in accordance with Borough Development Strategy 2017 Policy D5 and Policy L1 and Policy D2 (in part).

15. Vehicles leaving the site between 22.00 to 06.00 hours Monday to Saturday will not sound their horns within the service yard or service route. The testing of vehicle horns will not be undertaken until the vehicle has crossed the boundary at the exit gate onto Shelah Road.

REASON: To protect the amenities of nearby residents in accordance with Borough Development Strategy 2017 Policy D5 and Policy L1 and Policy D2 (in part).

16. The 3.75m acoustic fence as detailed in Drawing number 21-1883-011C (dated 01.11.22) and having a minimum surface density of 10kg/m² shall be installed before the approved use commences. The barrier shall be retained and maintained throughout the life of the development.

REASON: To protect the amenities of nearby residents in accordance with Borough Development Strategy 2017 Policy D5 and Policy L1 and Policy D2 (in part).

17. The rating level of sound emitted from any fixed plant and/or machinery associated with the development shall not exceed background sound levels by more than 5dB(A) between the hours of 0700-2300 at the nearest sound sensitive premises and shall not exceed the background sound level between 2300-0700 at the nearest sound sensitive premises. All measurements shall be made in accordance with the methodology of BS4142:2014+A1:2019 (Methods for rating and assessing industrial and commercial sound) and/or its subsequent amendments. Where access to the nearest sound sensitive property is not possible, measurements shall be undertaken at an appropriate location and corrected to establish the noise levels at the nearest sound sensitive property. Any deviations from the LA90 time interval stipulated above shall be agreed in writing with the Local Planning Authority.
REASON: To protect the amenities of nearby residents in accordance with Borough Development Strategy 2017 Policy D5 and Policy L1 and Policy D2 (in part).
18. Demolition or construction works shall only take place between the hours of 0700 hours and 1800 hours, Mondays to Fridays, and 0800 hours to 1700 hours on Saturdays and not at any time on Sundays or Public Holidays.
REASON: To protect the amenities of nearby residents in accordance with Borough Development Strategy 2017 Policy D5 and Policy L1 and Policy D2 (in part).
19. No above ground development shall commence until details of onsite renewable energy production, which shall provide at least 10% of the new developments needs, have been submitted to and approved in writing by the Local Planning Authority. The development shall thereafter be carried out in accordance with the approved details and retained for the lifetime of the development.
REASON: In accordance with the requirements of Policy ENV7 of the Black Country Core Strategy.
20. In the event that contamination is found at any time when carrying out the approved development which was not previously identified, development shall STOP on that part of the site affected by the unexpected contamination and it must be immediately reported in writing to the Local Planning Authority. An additional investigation and risk assessment must be undertaken and where remediation is necessary, a remediation scheme must be prepared, which shall be submitted to and approved in writing of the Local Planning Authority. Following approval, such remediation scheme shall be implemented on site in complete accordance with approved details unless

otherwise agreed in writing by the Local Planning Authority.

REASON: To ensure that the risks associated with any contamination are reduced to acceptable levels and that the health and wellbeing of future occupiers are protected and to ensure that the development complies with Borough Development Strategy 2017 Policy D3 and the NPPF.

21. No development shall commence (excluding demolition, site clearance and initial ground investigation works) until a detailed design and associated management and maintenance plan of surface water drainage for the site using sustainable drainage methods has been submitted to and approved in writing by the Local Planning Authority. The approved drainage system shall be implemented in accordance with the approved detailed design prior to the use of the building commencing.

REASON: In the interests of sustainability, reducing flood risk and run off and to comply with Adopted BCCS Policy ENV5.

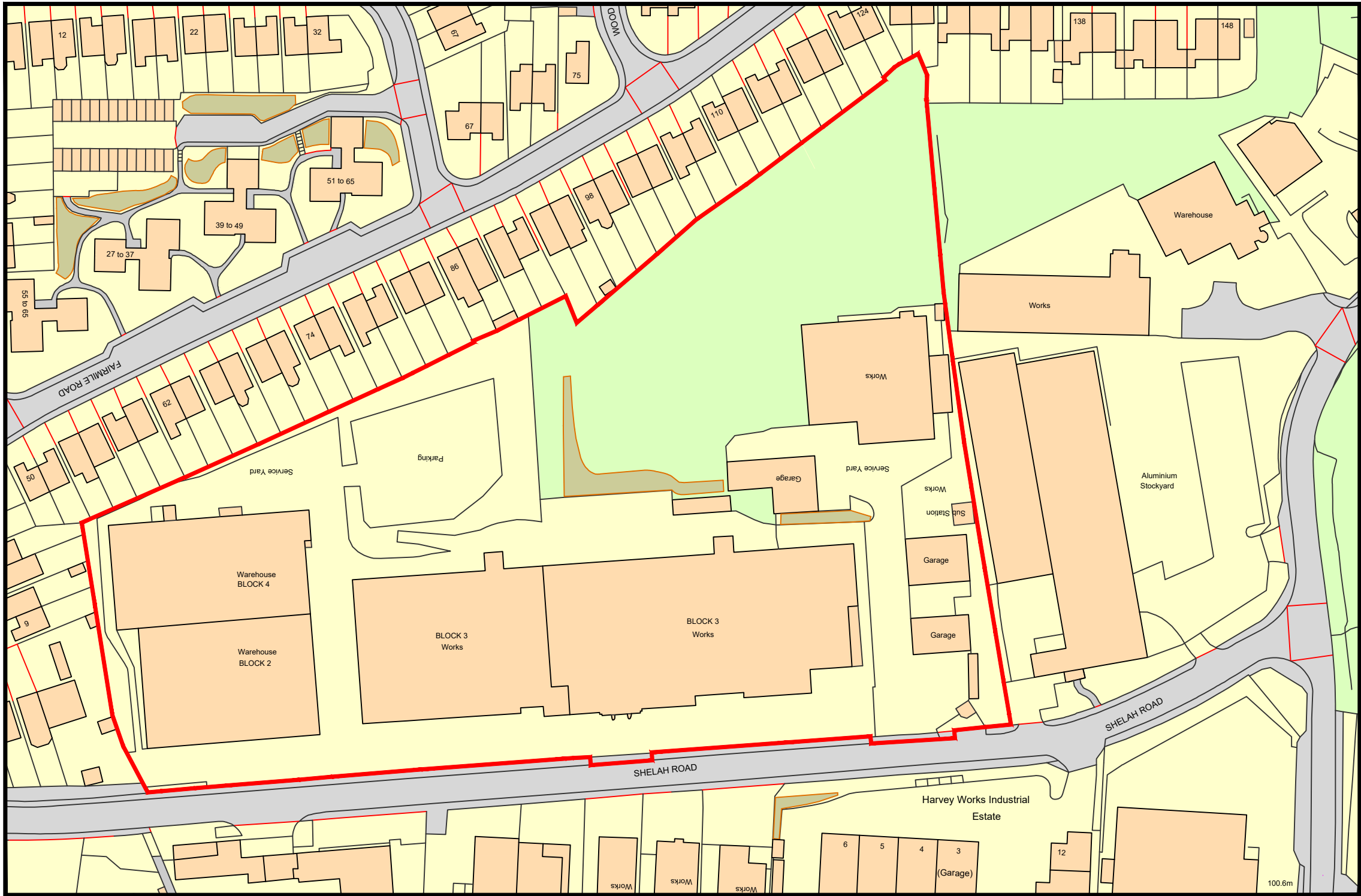
22. No development shall commence (excluding demolition, site clearance and initial ground works) until details of nature conservation enhancement works have been submitted to and approved in writing by the Local Planning Authority. The nature conservation enhancement works shall thereafter be provided in accordance with the approved details prior to first occupation of the development and be maintained for the life of the development.

REASON: In order to enhance, encourage and protect the nature conservation value of the site and in accordance with BCCS Policies ENV1, CSP3 and DEL1 and Borough Development Strategy 2017 Policy S21 and Policy S19 (in part).

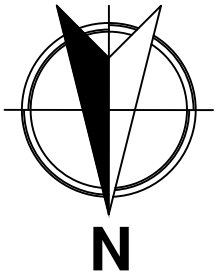
This detail is required pre commencement (excluding demolition, site clearance and initial ground works) in that the required works may need to be incorporated into buildings on the site.

23. The landscaped areas shall be retained in the form shown on the approved plan throughout the life of the development and shall not be used for any other purpose, unless otherwise agreed in writing by the Local Planning Authority.

REASON: To preserve the character and visual amenities of the area, in accordance with BCCS Policies CSP4, ENV 2 , ENV3 and DEL1 and Borough Development Strategy 2017 Policy S6 and Policy L1 and Policy D2.



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LOCATION PLAN - AS EXISTING

Scale - 1:1250

SITE AREA: 2.902 ACRES



1 : 1250

A	18.11.22	General Planning Application Updated	PS	SC
rev.	date		drawn	chk'd



Gould Singleton Architects
Earls Way, Halesowen, West Midlands, B63 3HR
Tel: 0121 550 0359 Fax: 0121 550 8088
e-mail: info@gould-singleton.co.uk

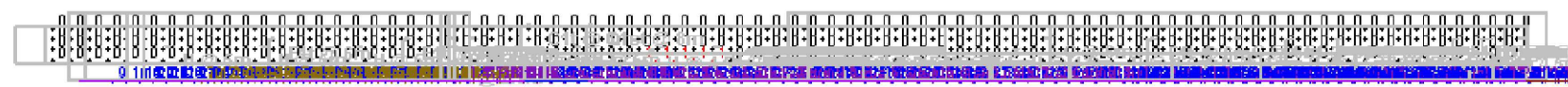
PLANNING

1:1250@A3	Hayley Group Limited
01.11.2022	Shelah Road
	Halesowen
	West Midlands - B63 3XL
PS	Location Plan - As Existing
checked	
number	
21-1883/001A	

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Schedule							
Symbol	Label	Quantity	Manufacturer	Catalog Number	Description	Wattage	
	B1	3	HOLOPHANE EUROPE LIMITED	1 x OSK0 1.1A154 FW on 8in Column	D-Series 0 Area luminaire	88	
	C1st	1	HOLOPHANE EUROPE LIMITED	DS00 1.1A154 FW BLS on 8in Column and Backlight Sheet 3 Degree Tilt Bracket	D-Series 0 Area luminaire	81	
	C1t	1	HOLOPHANE EUROPE LIMITED	1 x OSK0 1.1A154 FW on 8in Column 3 Degree Tilt Bracket	D-Series 0 Area luminaire	81	
	D1	3	HOLOPHANE EUROPE LIMITED	1 x OSK0 1.1A154 FW on 8in Column	D-Series 0 Area luminaire	107	
	Et1	3	HOLOPHANE EUROPE LIMITED	DS00 1.1A154 BLC on 8in Column 3 Degree Tilt Bracket	D-Series 0 Area luminaire	107	
	F1	4	HOLOPHANE EUROPE LIMITED	1 x OSK0 1.1A154 BLC on 8in Column	D-Series 0 Area luminaire	81	
	G	5	HOLOPHANE EUROPE LIMITED	WAP 1.1A154 FW at Height Shown	WALLPACK	81	
	H	23	HOLOPHANE EUROPE LIMITED	WAP 1.1A154 FW at Height Shown	WALLPACK	90	
	I	1	HOLOPHANE EUROPE LIMITED	DWL 1.1A154 FW W01 at Height Shown	Downer 0 Wall	10.3	
	J	19	Holophane Europe	5887	Vertical Linear at Height Shown	WFL	25

Statistics				
Description	Symbol	Avg	Min	Min/Avg
Car Park A - 20/0.25Uo	+	23 lux	6 lux	0.26
Car Park B - 20/0.25Uo	+	21 lux	9 lux	0.43
Car Park D and Side - 20/0.25Uo	+	21 lux	6 lux	0.29
Car Park Rear - 20/0.25Uo	+	21 lux	5 lux	0.24
Loading Yard - 20/0.40Uo	+	24 lux	10 lux	0.42
Service Yard Block 4 - 20/0.40Uo	+	24 lux	4 lux	0.17
Access Roads 0 20/0.40Uo	+	21 lux	9 lux	0.43



Statistics			
Description	Symbol	Avg	Max
Vertical Spill to Residents	+	0 lux	0 lux
Vertical Spill to Residents, Building	+	0 lux	1 lux



SITE PLAN AS PROPOSED

SCALE 1:500

Plan View

Scale: 1:500

Notes :

- Electrical services shall be installed 600mm deep, in black rigid duct, with electrical meter base at 250mm below the finished ground level. Access chambers shall be installed every 35m or at any change of direction. 200, 150mm ducts shall be installed. Indicative positions have been shown in some areas.
- All services to be installed to BS7671 18th Edition, including all amendments.
- The drawing is indicative only, to show the works that are required for tender purposes only and is not to be used as an installation drawing.
- Duct routes are indicative only. The contractor shall allow to install suitably sized ducts and quantities to suit site requirements and satisfy tenant specifications as indicated the services specification, Part C 2.29 and 2.31.
- All incoming service positions to be agreed with the design team and tenants.
- All services to be installed in combined trenches where possible.
- All below ground services are to be installed c/w service specific traceable marker/warning tape located at a suitable height above associated service, throughout the entire length of service.

PL2 Updated to comments	21.03.23	EA	JW
Rev Details	Date	Dwn	App

BENNETT WILLIAMS
BUILDING SERVICES CONSULTANTS

106 Drons Green Road
Dudley,
West Midlands
DY2 7DH

Tel | 01384 235 222
Email | office@bennettwilliams.co.uk
Web | www.bennettwilliams.co.uk

Client :
Hayley Group

Project :
Hayley Group, Halesowen

Description :
Indicative External Lighting Services

Date : 28/10/2022	Dwn : RH
Scale : 1:500 @ A0	Ok'd : JW
	App : JW
Drawing No. : 11884-E-001	Rev. : PL2

PLANNING



PLANNING

A 18.11.22		General Planning Application Updated		PS		SC	
rev. date				drawn chk'd			
		Gould Singleton Architects Earls Way, Halesowen, West Midlands, B63 3HR		Tel: 0121 550 0359 Fax: 0121 550 8088 e-mail: info@gould-singleton.co.uk			
NTS @ A3		Hayley Group Limited					
date		Shelah Road					
01.11.2022		Halesowen					
drawn		West Midlands - B63 3XL					
PS		Block Plan & Aerial Images					
checked							
number							
1-1883-002A							



TOPOGRAPHICAL SURVEY AS EXISTING SCALE 1:500

NB: Buildings adjacent to application site are illustrative only

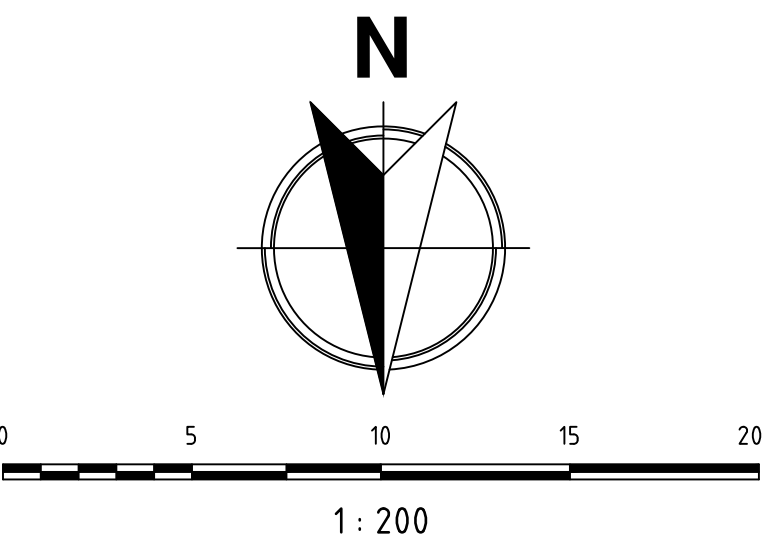
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number	drawn	date	1:500@A1	checked	PS	SC	21.1883.003A		A 18.11.22	General Planning Application Updated		PS	SC		
										rev.	date	drawn chk'd			
									Gould Singleton Architects		Tel: 0121 550 0359 Fax: 0121 550 8088 e-mail: info@gould-singleton.co.uk				
									Earls Way, Halesowen, West Midlands, B63 3HR						
									Hayley Group Limited						
									Shelah Road						
									Halesowen						
									West Midlands - B63 3XL						
									Topographical Survey As Existing						
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West Midlands - B63 3XL



A		18.11.22		General Planning Application Updated		PS		SC	
rev.		date				drawn		chk'd	

PLANNING



Gould Singleton Architects

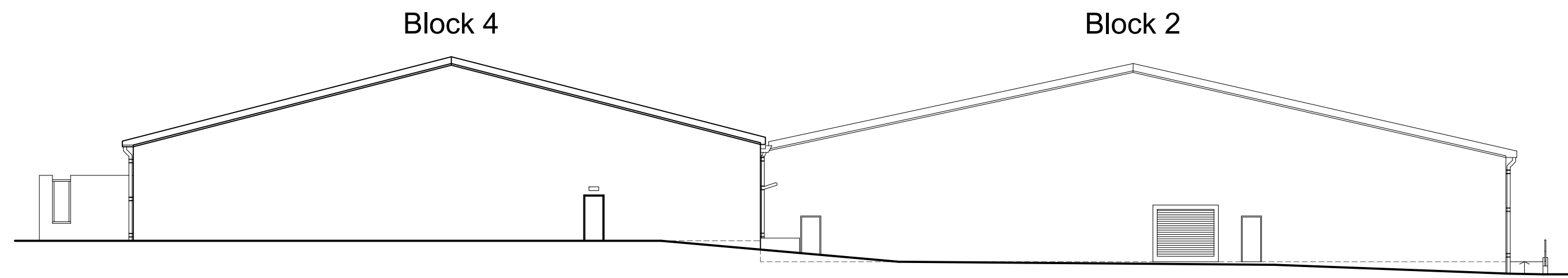
Earls Way, Halesowen, West Midlands, B63 3HR

Tel 0121 550 0359 Fax 0121 550 8088

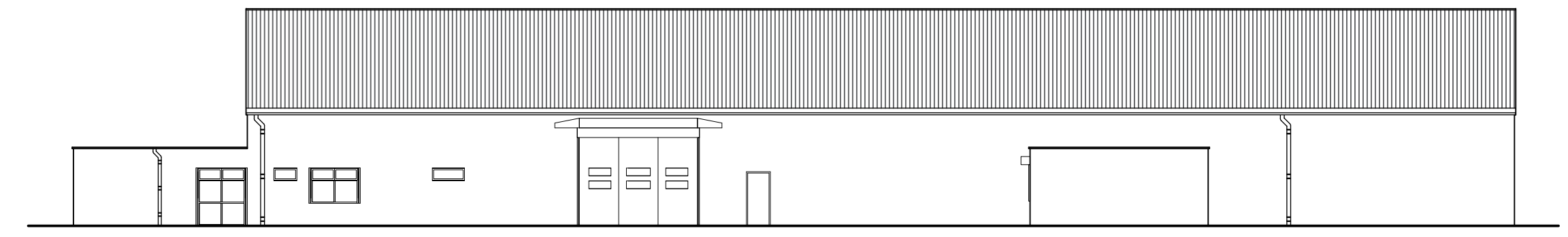
e-mail: info@gould-singleton.co.uk

number	drawn	date	scales	
	checked			
	PS	SC		
21.1883.004A				<p>Hayley Group Limited</p> <p>Shelah Road</p> <p>Halesowen</p> <p>West Midlands - B63 3XL</p> <p>Site Sections As Existing</p>

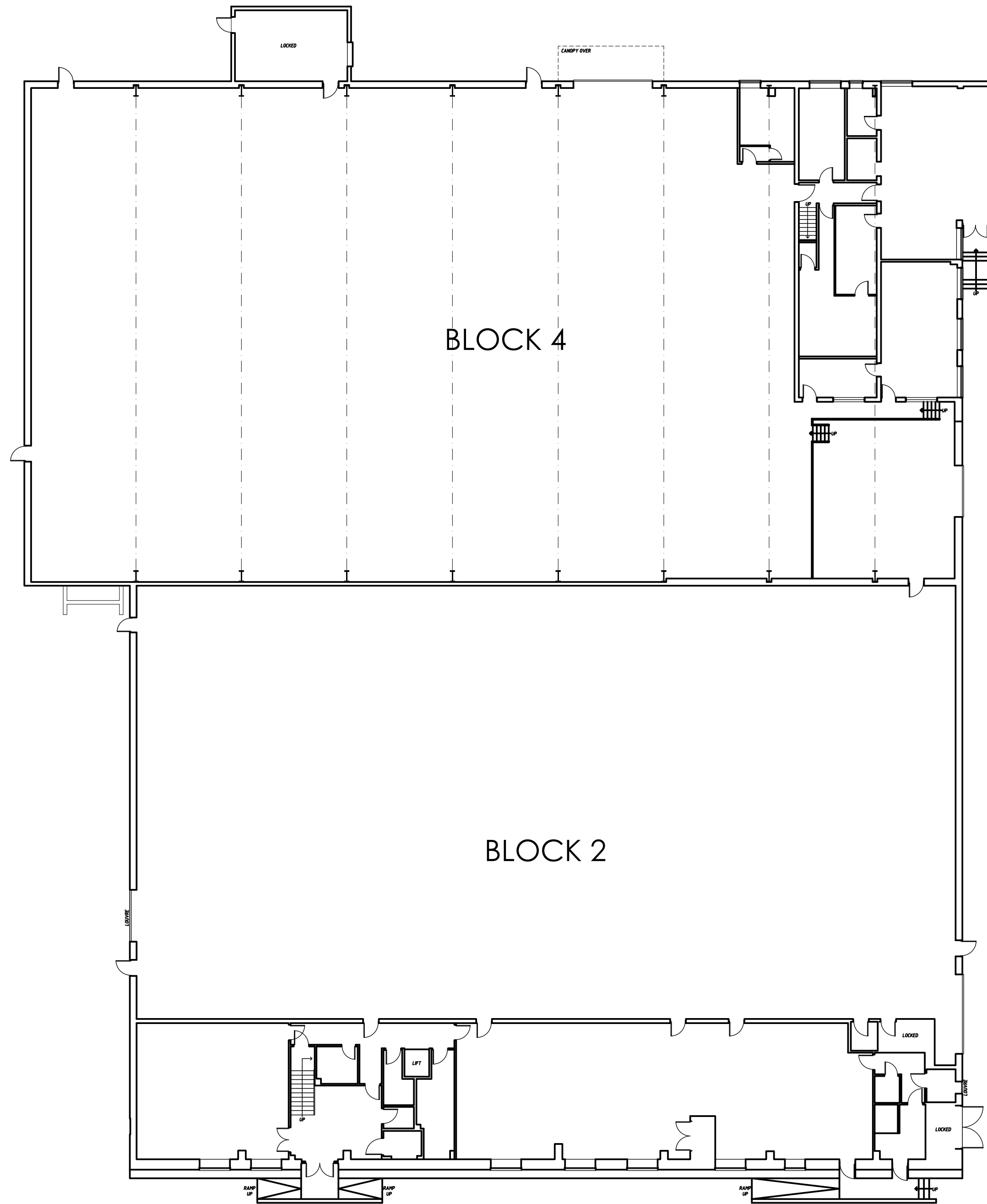
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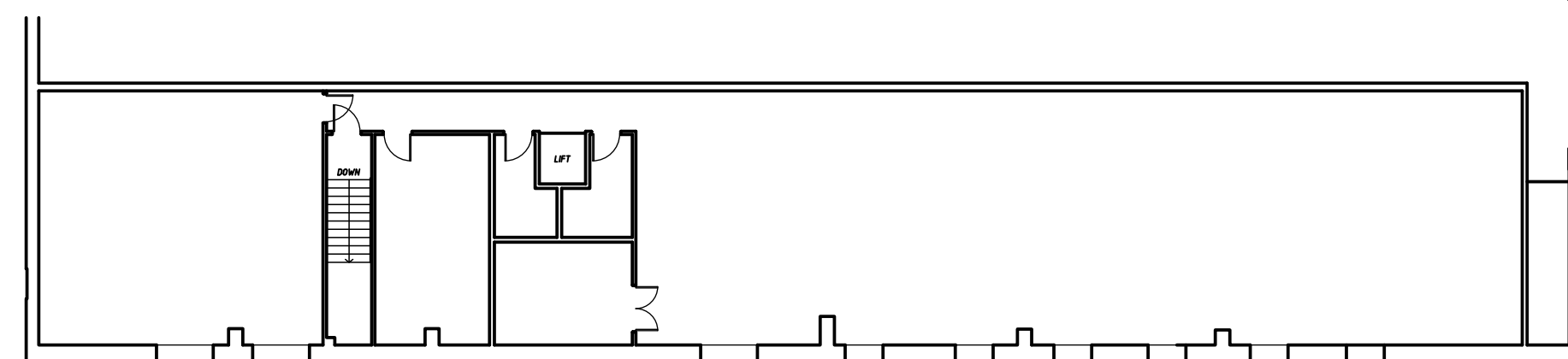
BLOCK 2 & 4 - SIDE ELEVATION - 1:200
VIEW 14



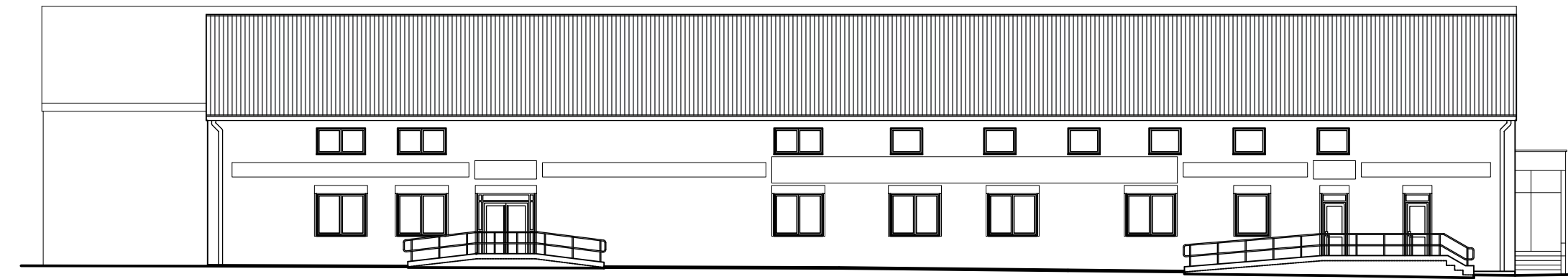
BLOCK 2 & 4 - REAR ELEVATION - 1:200
VIEW 15



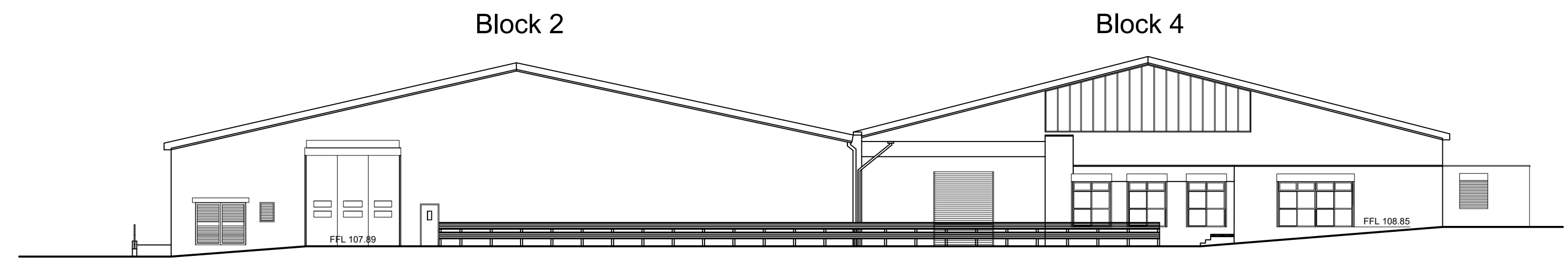
BLOCK 2 & 4 - GROUND FLOOR PLAN - 1:200



BLOCK 2- FIRST FLOOR PLAN - 1:200

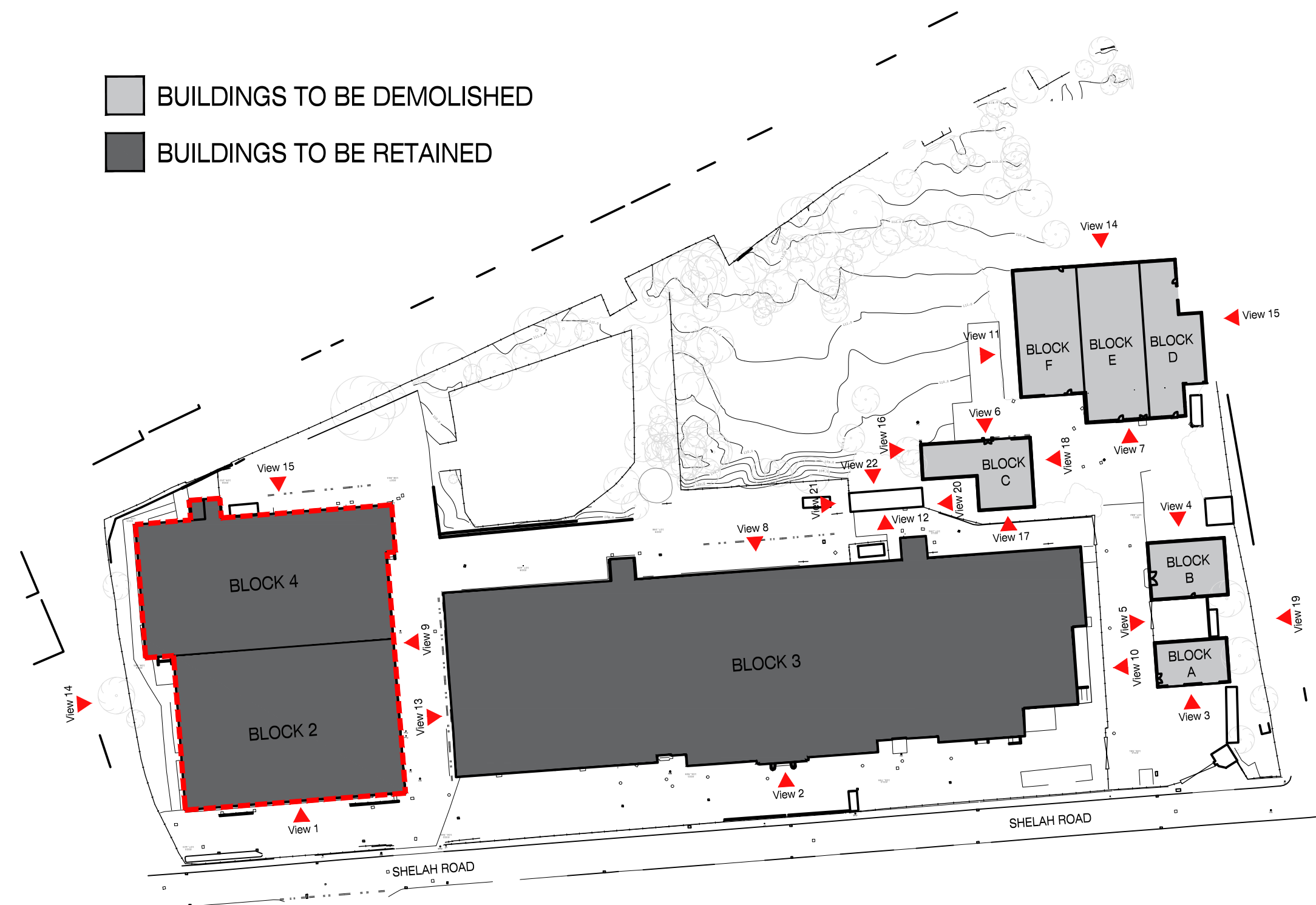


BLOCK 2 - FRONT ELEVATION - 1:200
VIEW 1



BLOCK 2 & 4 - SIDE ELEVATION - 1:200
VIEW 9

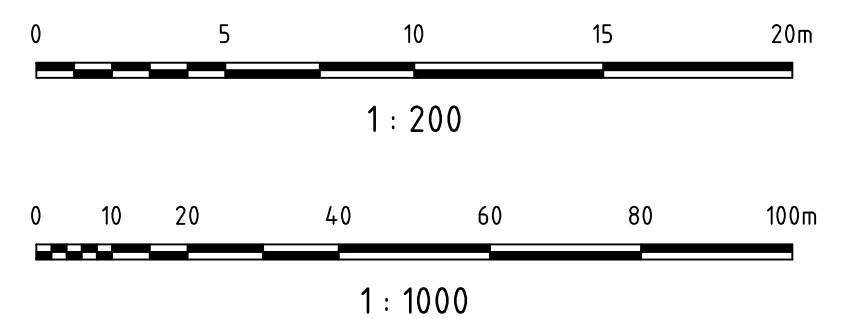
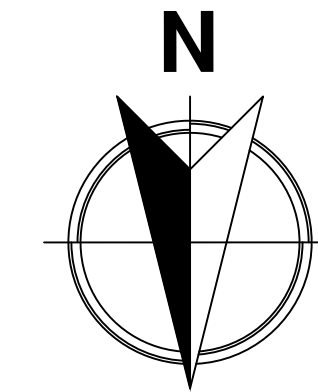
- BUILDINGS TO BE DEMOLISHED
- BUILDINGS TO BE RETAINED



EXISTING SITE PLAN - 1:1000

Note: Topo Drawings provided from 3rd party. Full survey and dimension check required.

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A 18.11.22 General Planning Application Updated PS SC

rev. date drawn chkd

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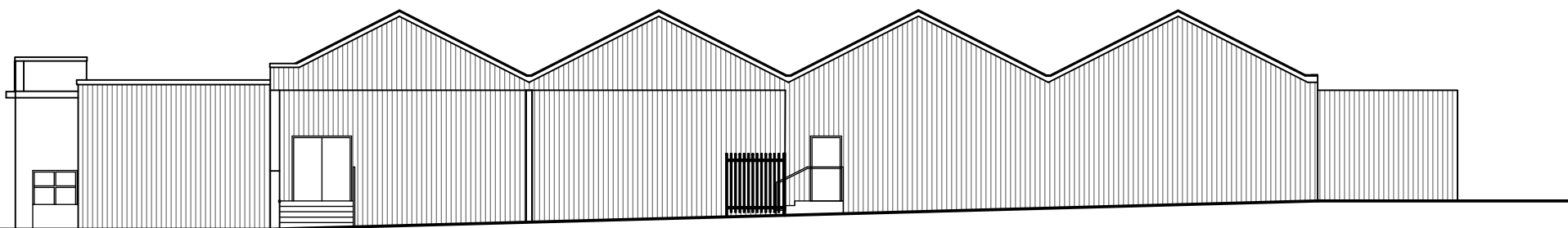
Varies@A1
01.11.2022

PS SC
checked

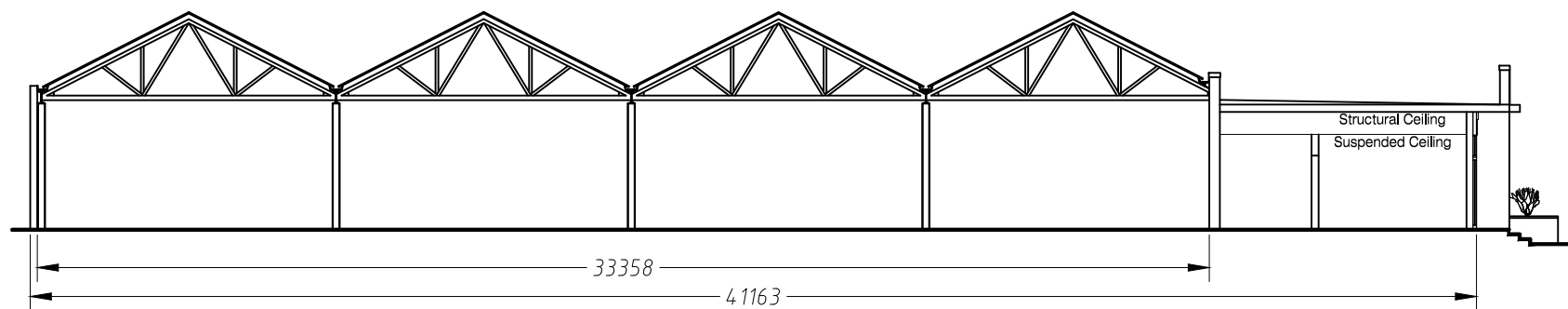
21-1883/005A
Blocks 2 & 4 Plans & Elevations As Existing

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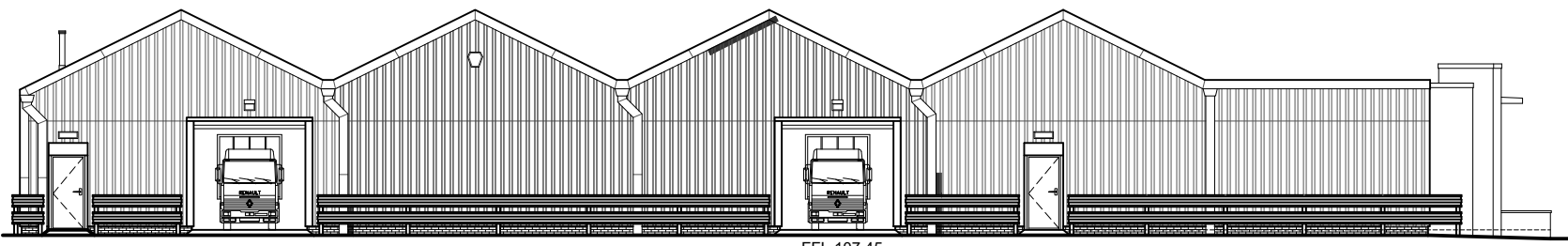
PLANNING



BLOCK 3 - SIDE ELEVATION D - 1:200 - VIEW 10

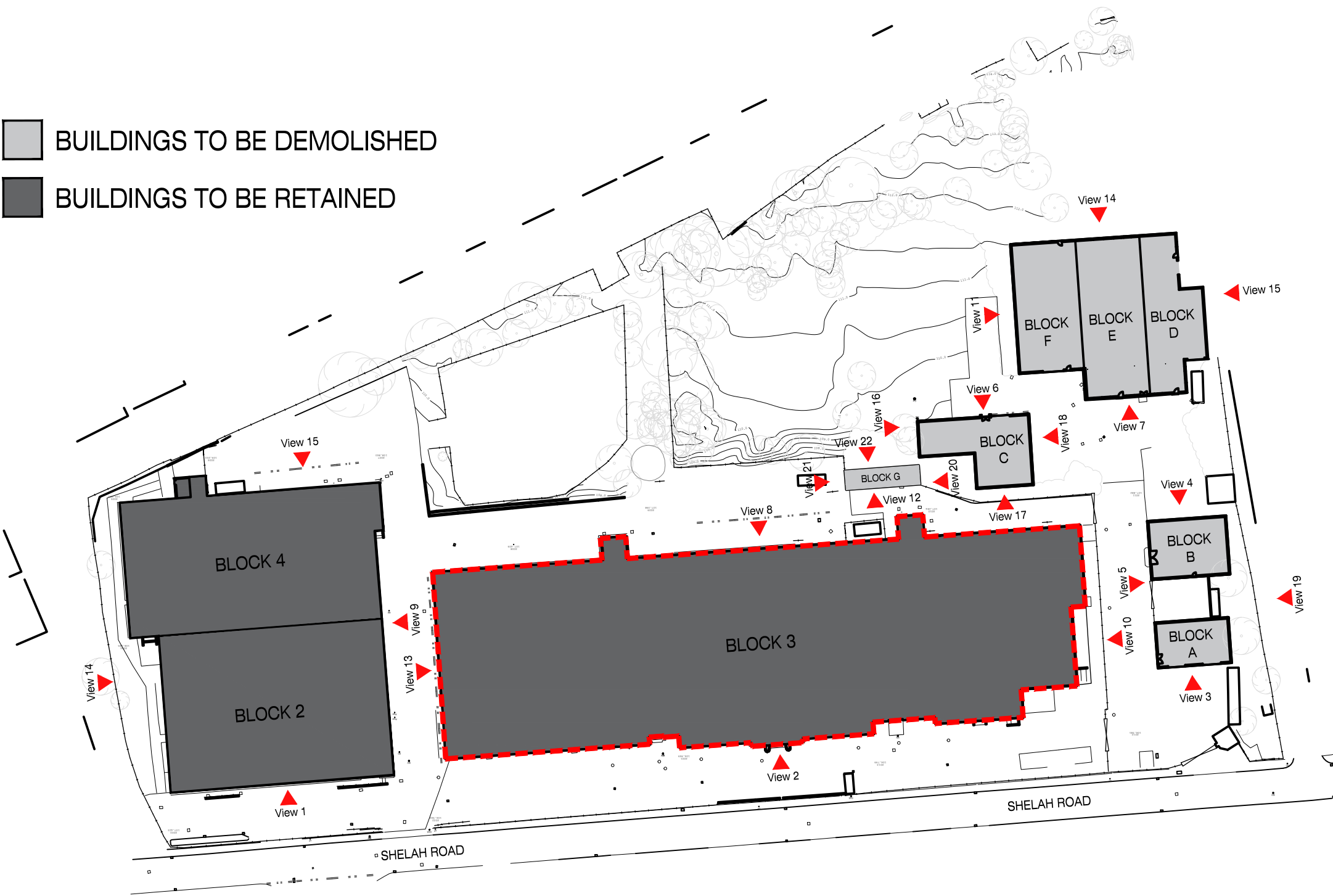


BLOCK 3 - SECTION A-A - 1:200

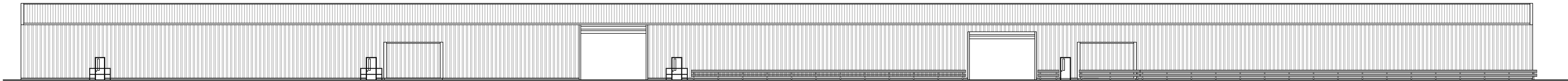


BLOCK 3 - SIDE ELEVATION B - 1:200 - VIEW 13

- BUILDINGS TO BE DEMOLISHED
- BUILDINGS TO BE RETAINED



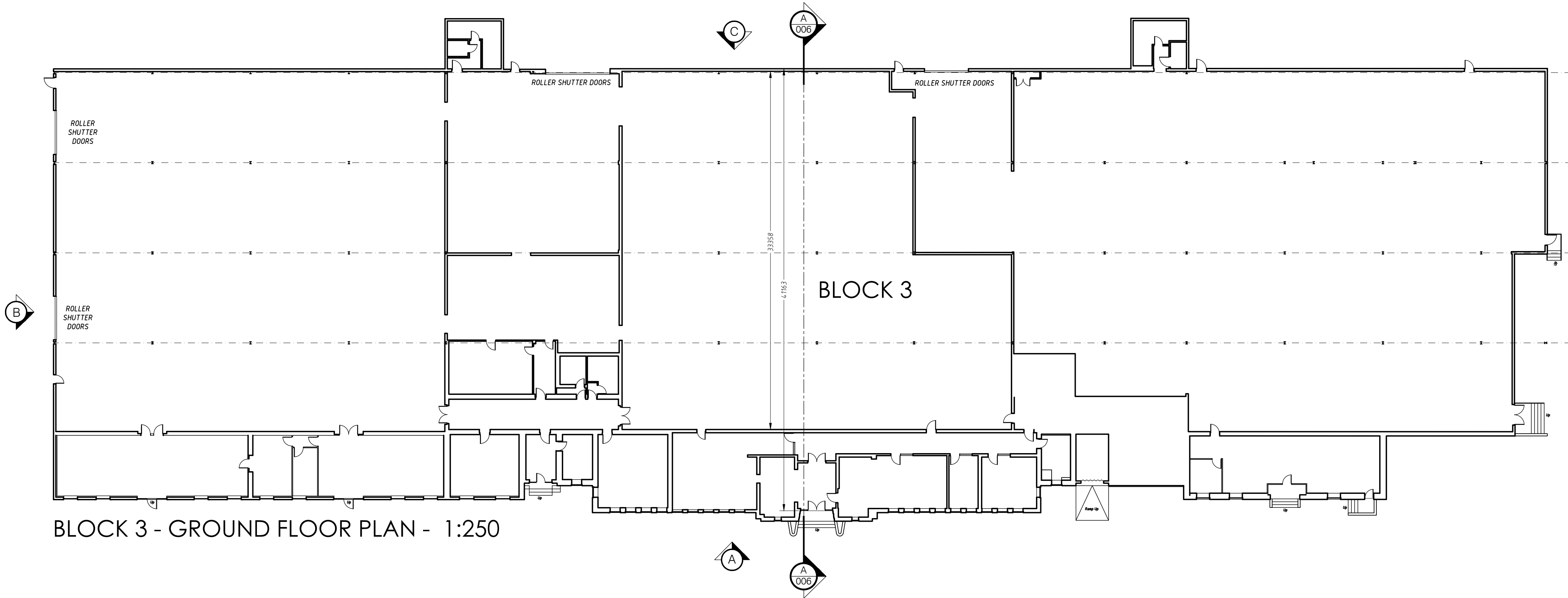
EXISTING SITE PLAN - 1:1000



BLOCK 3 - REAR ELEVATION C - 1:200 - VIEW 8



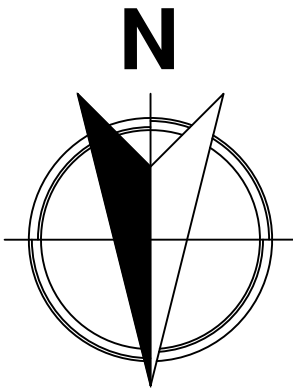
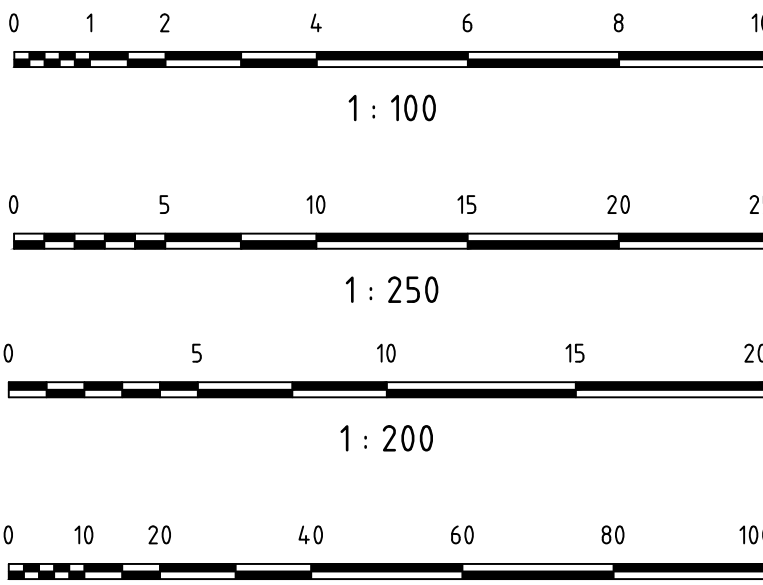
BLOCK 3 - FRONT ELEVATION A - 1:200 - VIEW 2



BLOCK 3 - GROUND FLOOR PLAN - 1:250

Note: Topo Drawings provided from 3rd party. Full survey and dimension check required.

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rev. | date | drawn | chk'd |

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Varies@A1 Hayley Group Limited

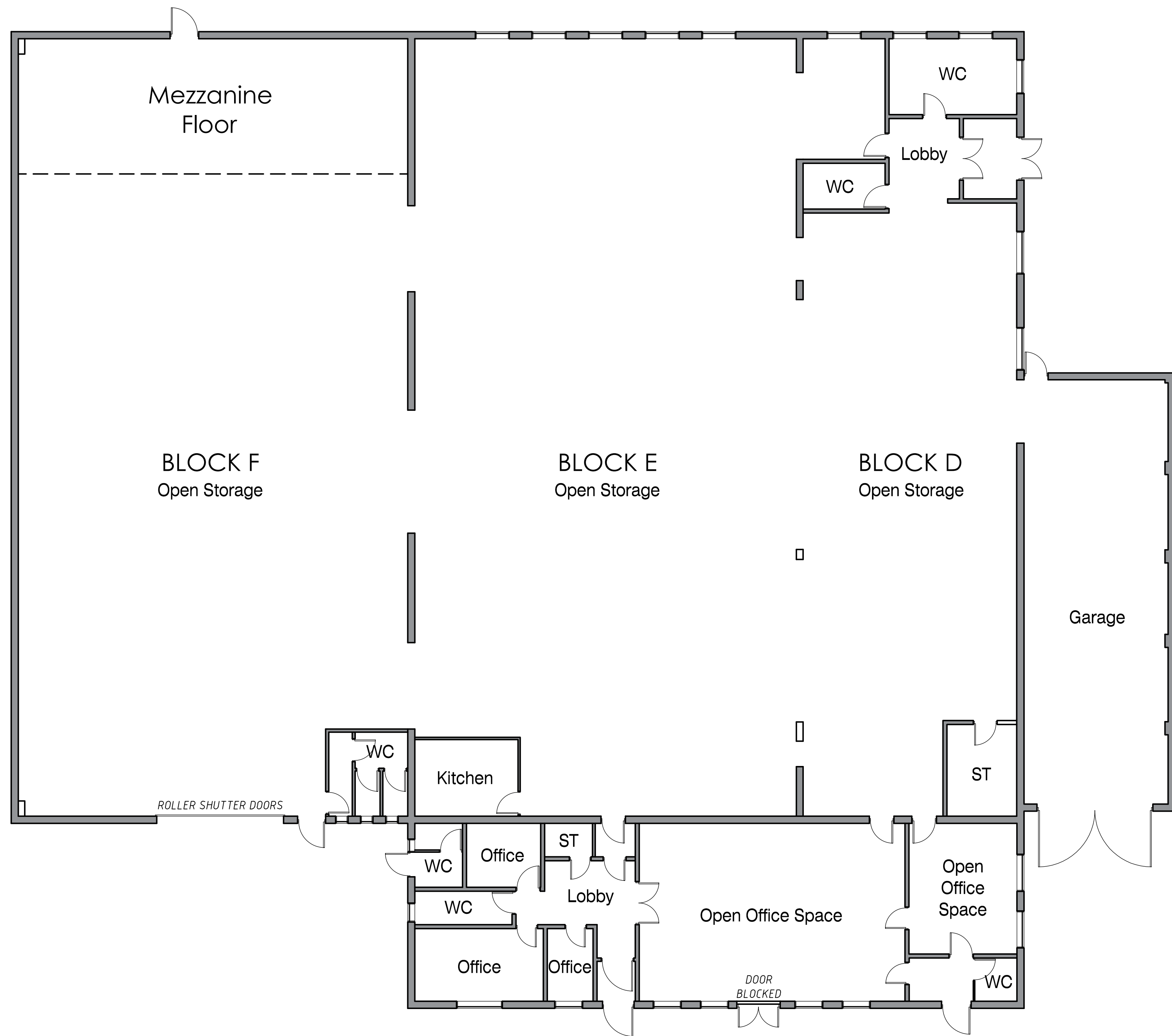
01.11.2022 Halesowen

West Midlands

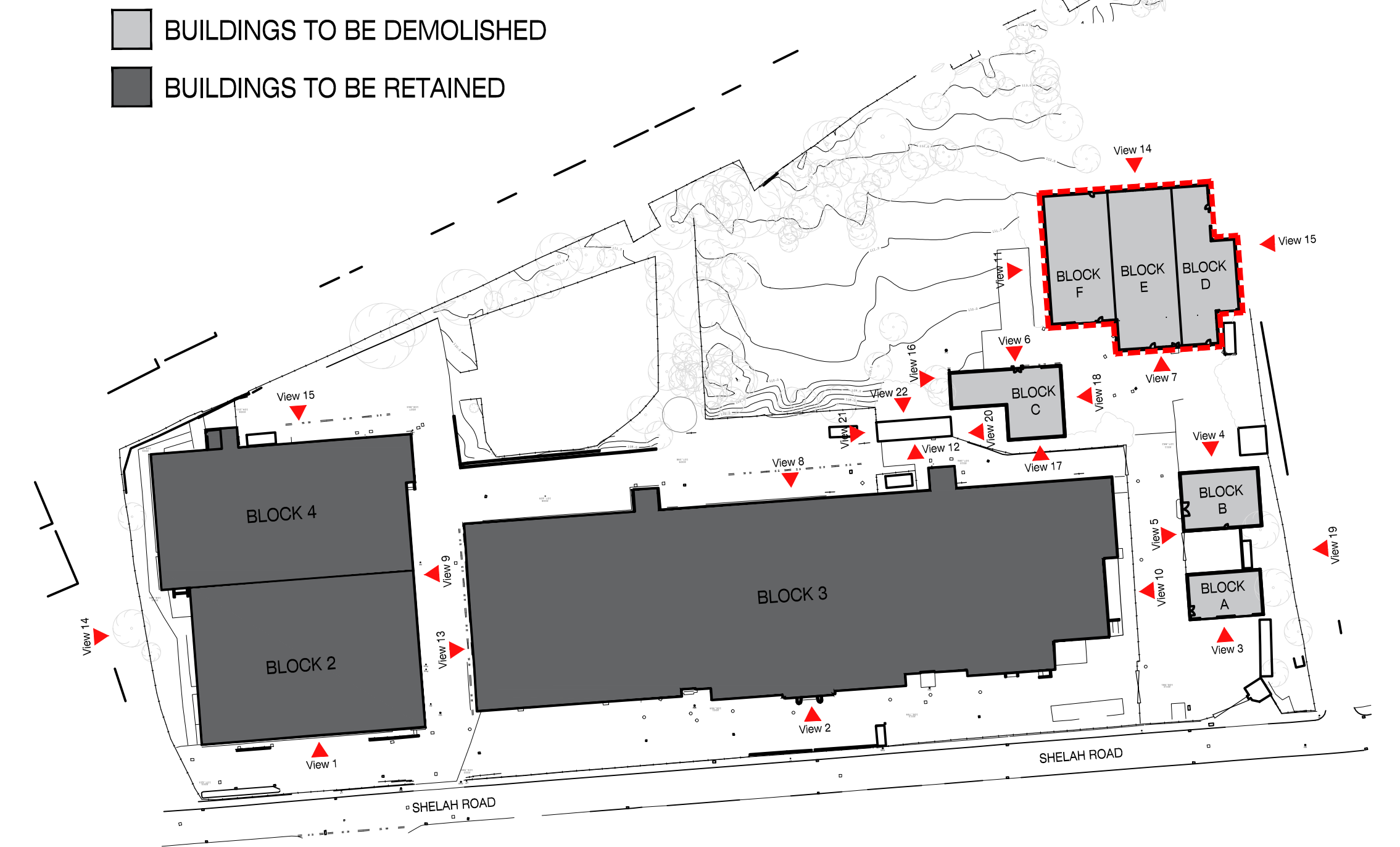
PS SC Block 3 Plan, Elevations & Section As Existing

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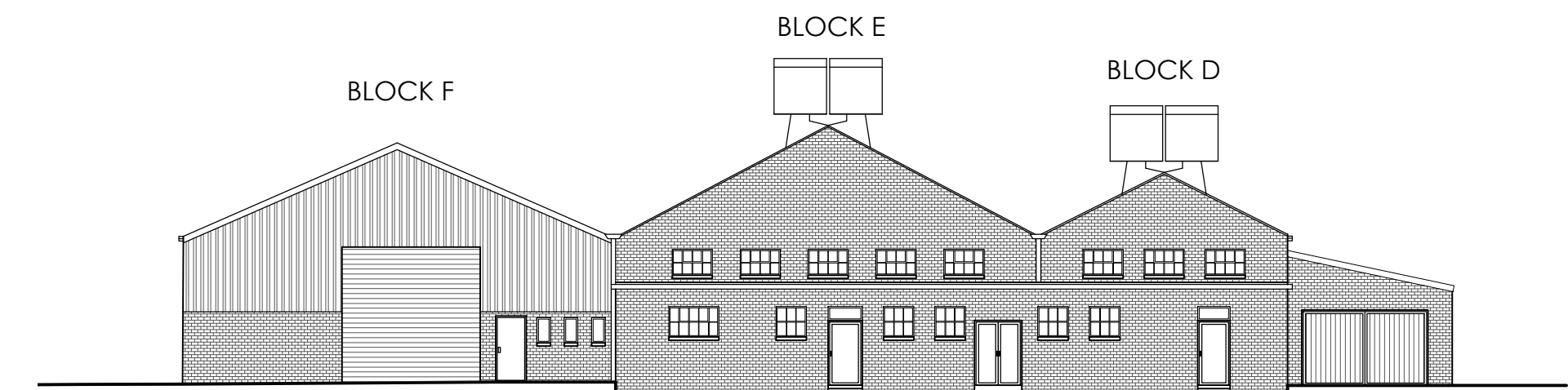
PLANNING



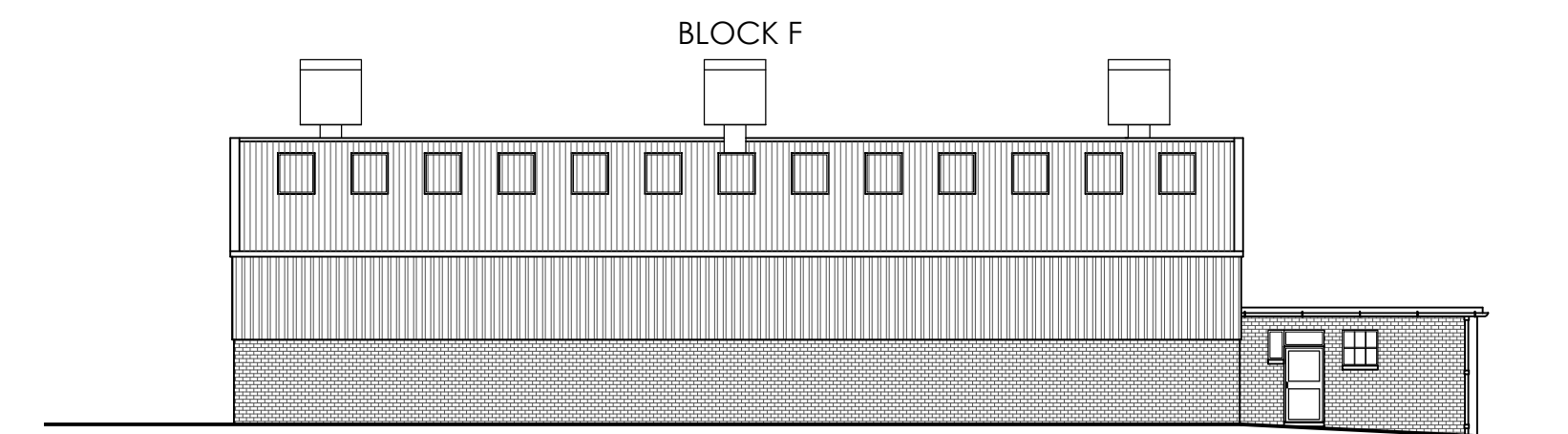
BLOCKS D, E & F - EXISTING FLOOR PLAN - 1:100



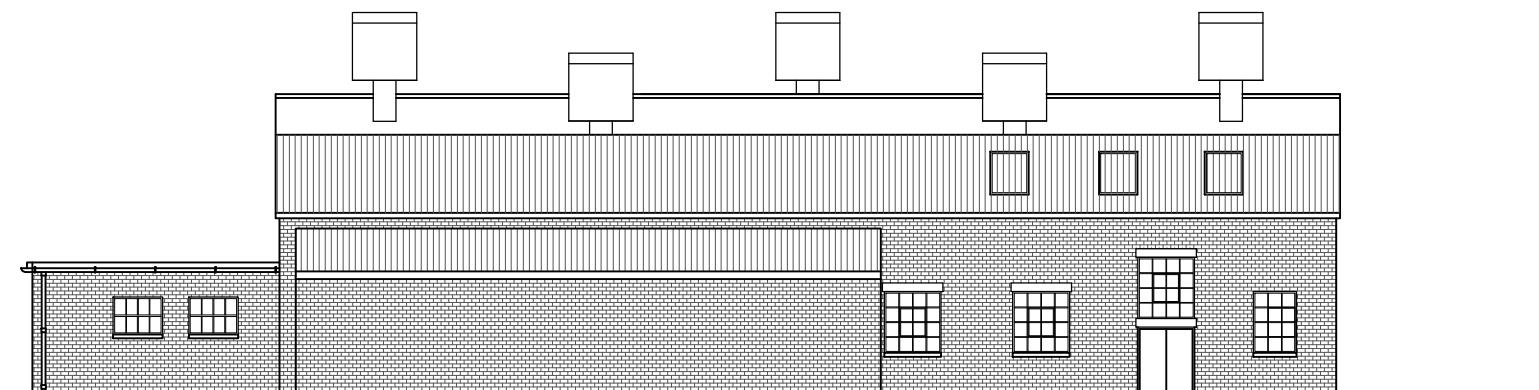
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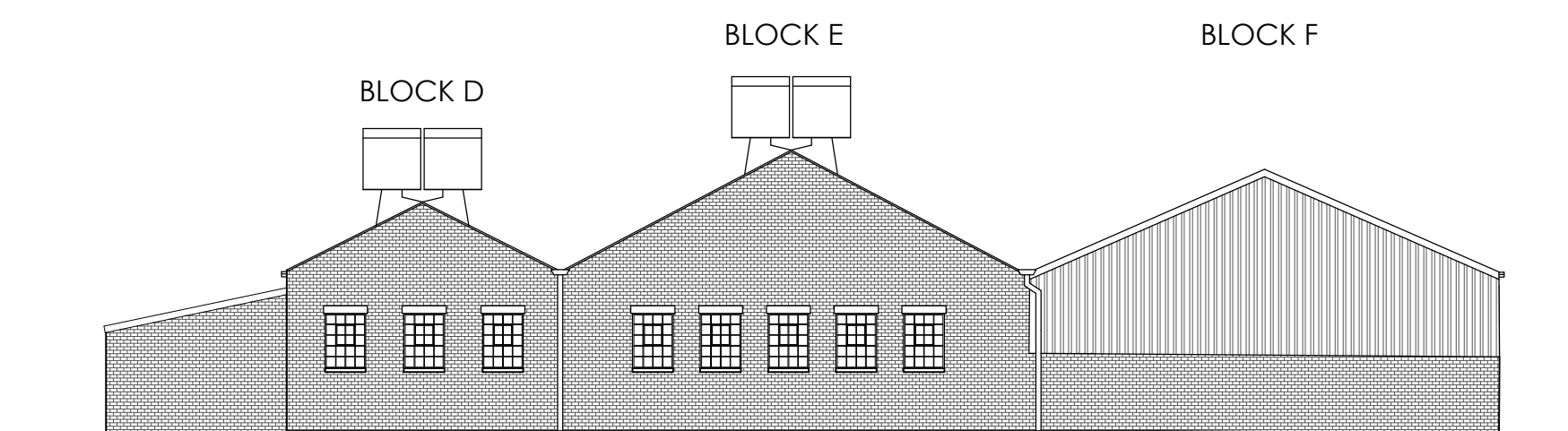
BLOCKS D, E & F - FRONT ELEVATION - 1:200 - VIEW 7



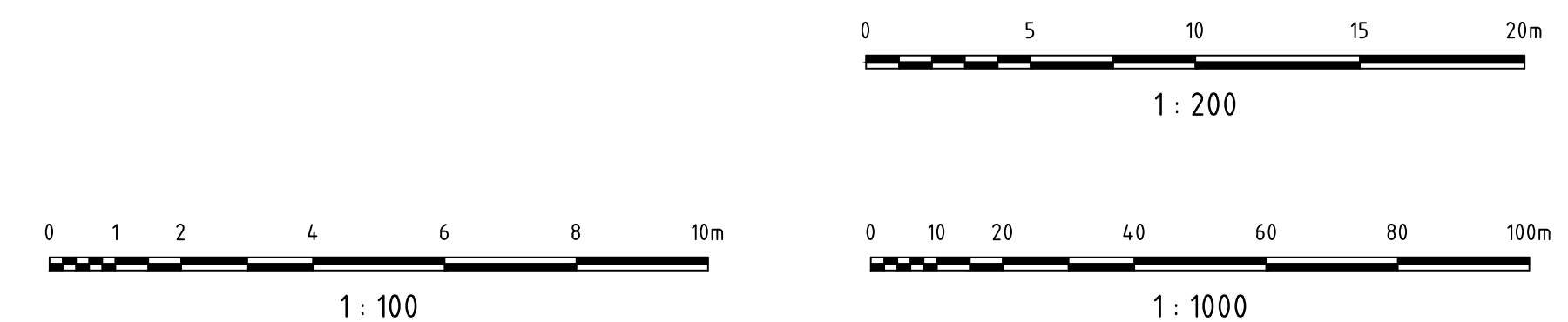
BLOCKS D, E & F - SIDE ELEVATION - 1:200 - VIEW 11



BLOCKS D - SIDE ELEVATION - 1:200 - VIEW 15



BLOCKS D, E & F - REAR ELEVATION - 1:200 - VIEW 14



Note: Topo Drawings provided from 3rd party. Full survey and dimension check required.

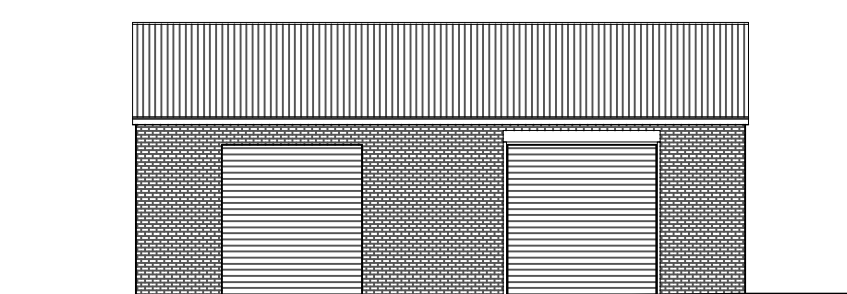
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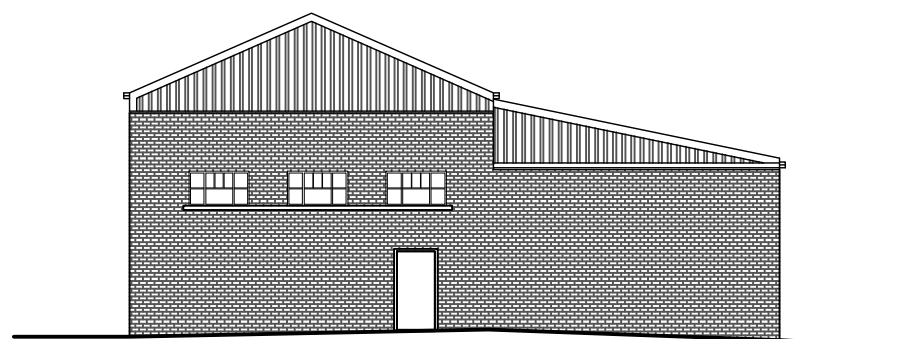
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A	18.11.22			
drawn	01.11.2022			
checked	PS		SC	
number	21-1883/007A			



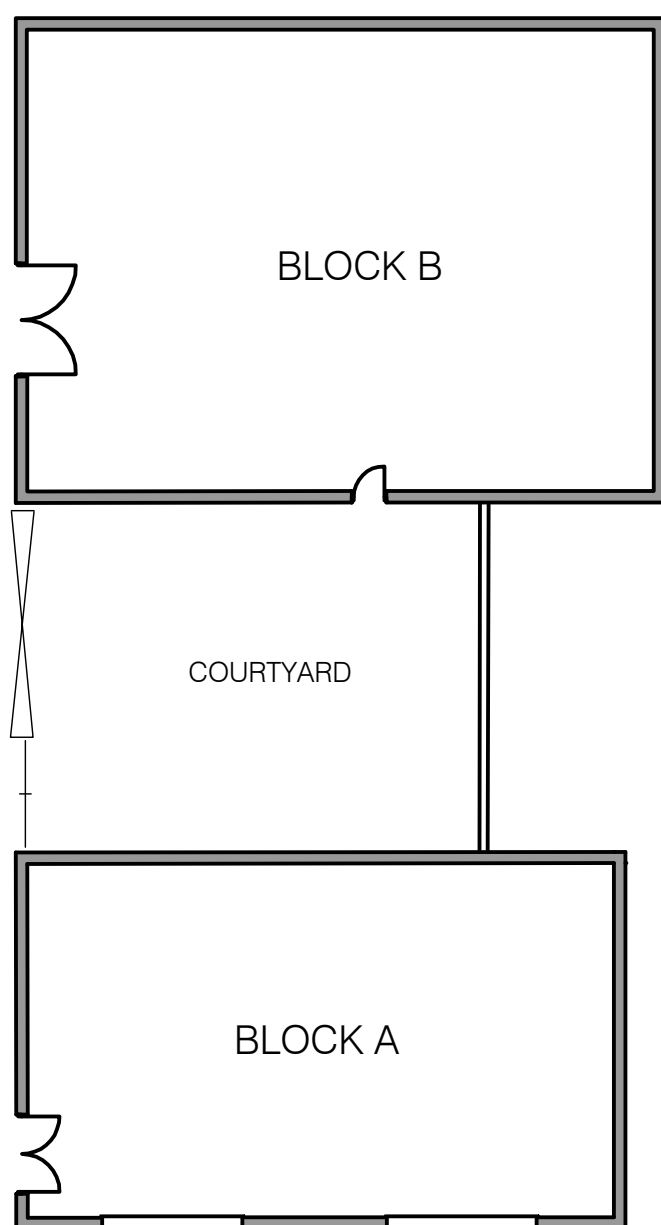
Gould Singleton Architects
 Earls Way, Halesowen, West Midlands, B63 3HR
 Hayley Group Limited
 Shelah Road
 Halesowen
 West Midlands
 Blocks D, E & F Plan & Elevations As Existing
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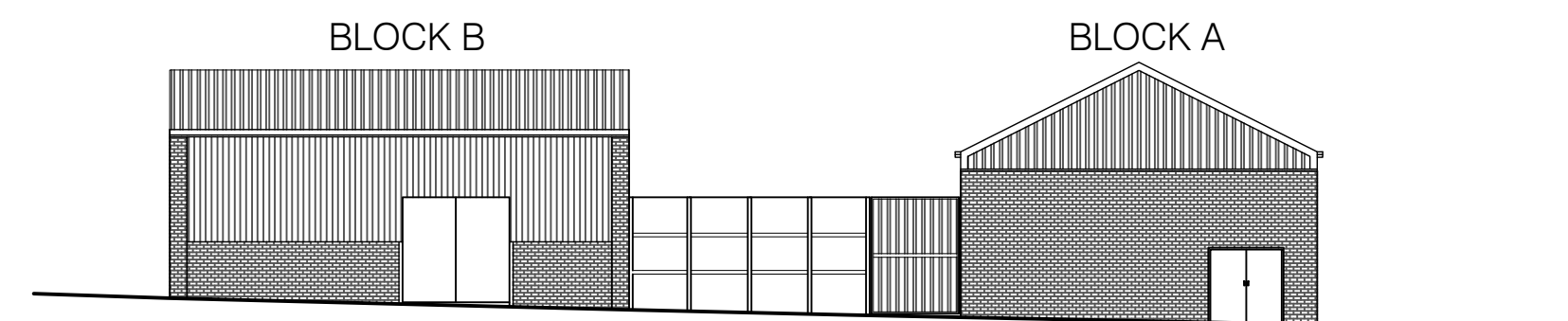
BLOCK A - FRONT
ELEVATION - 1:200
VIEW 3



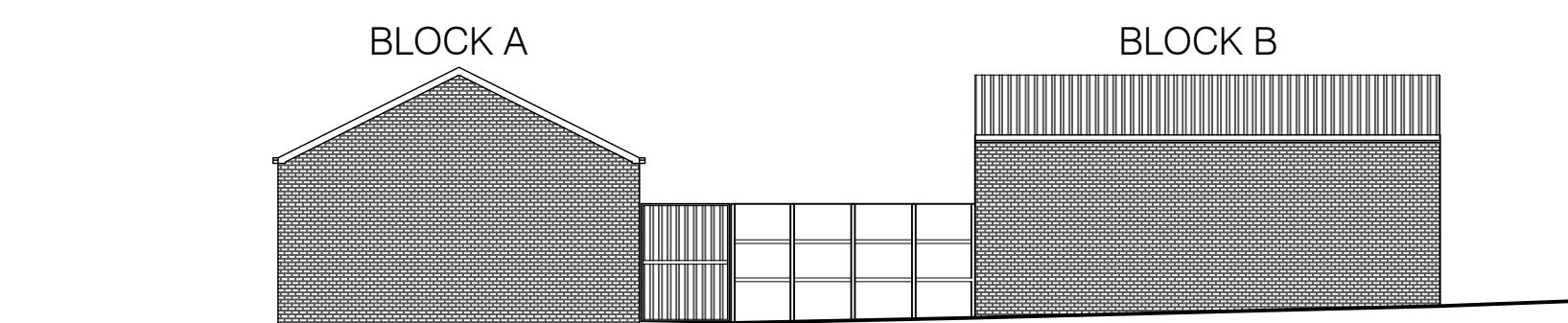
BLOCK B - SIDE
ELEVATION 1:200
VIEW 4



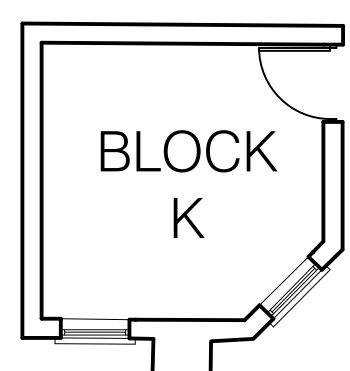
BLOCK A - EXISTING
FLOOR PLAN - 1:200



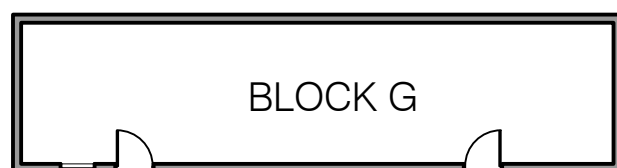
BLOCKS A & B - SIDE
ELEVATION - 1:200
VIEW 5



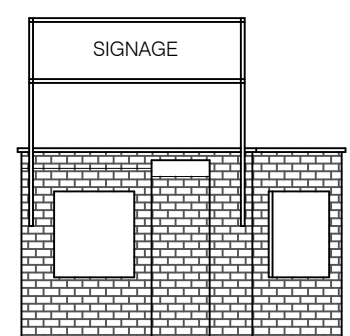
BLOCKS A & B - REAR
ELEVATION - 1:20
VIEW 19



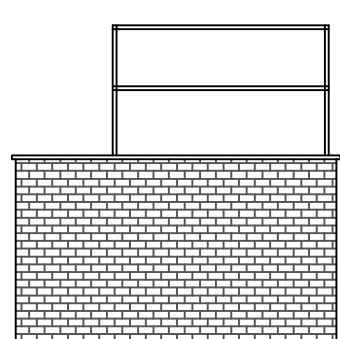
BLOCK K - PLAN
1:100



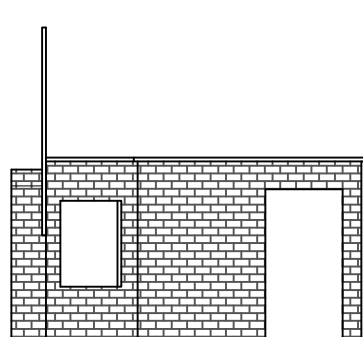
BLOCK G - EXISTING
FLOOR PLAN - 1:200



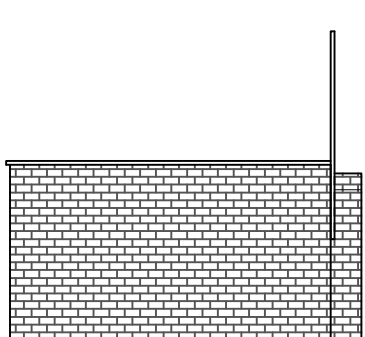
BLOCK K - FRONT
ELEVATION - 1:100
VIEW A



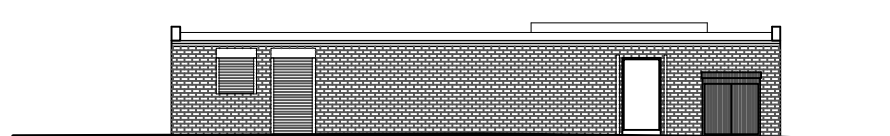
BLOCK K - REAR
ELEVATION - 1:100
VIEW C



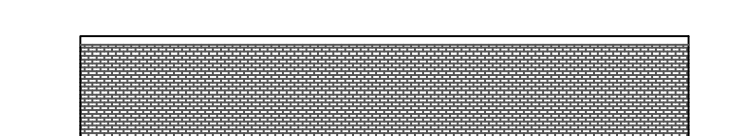
BLOCK K - SIDE
ELEVATION - 1:100
VIEW D



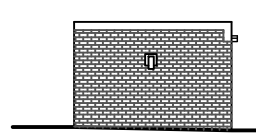
BLOCK K - SIDE
ELEVATION - 1:100
VIEW B



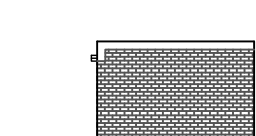
BLOCK G - FRONT
ELEVATION - 1:200
VIEW 12



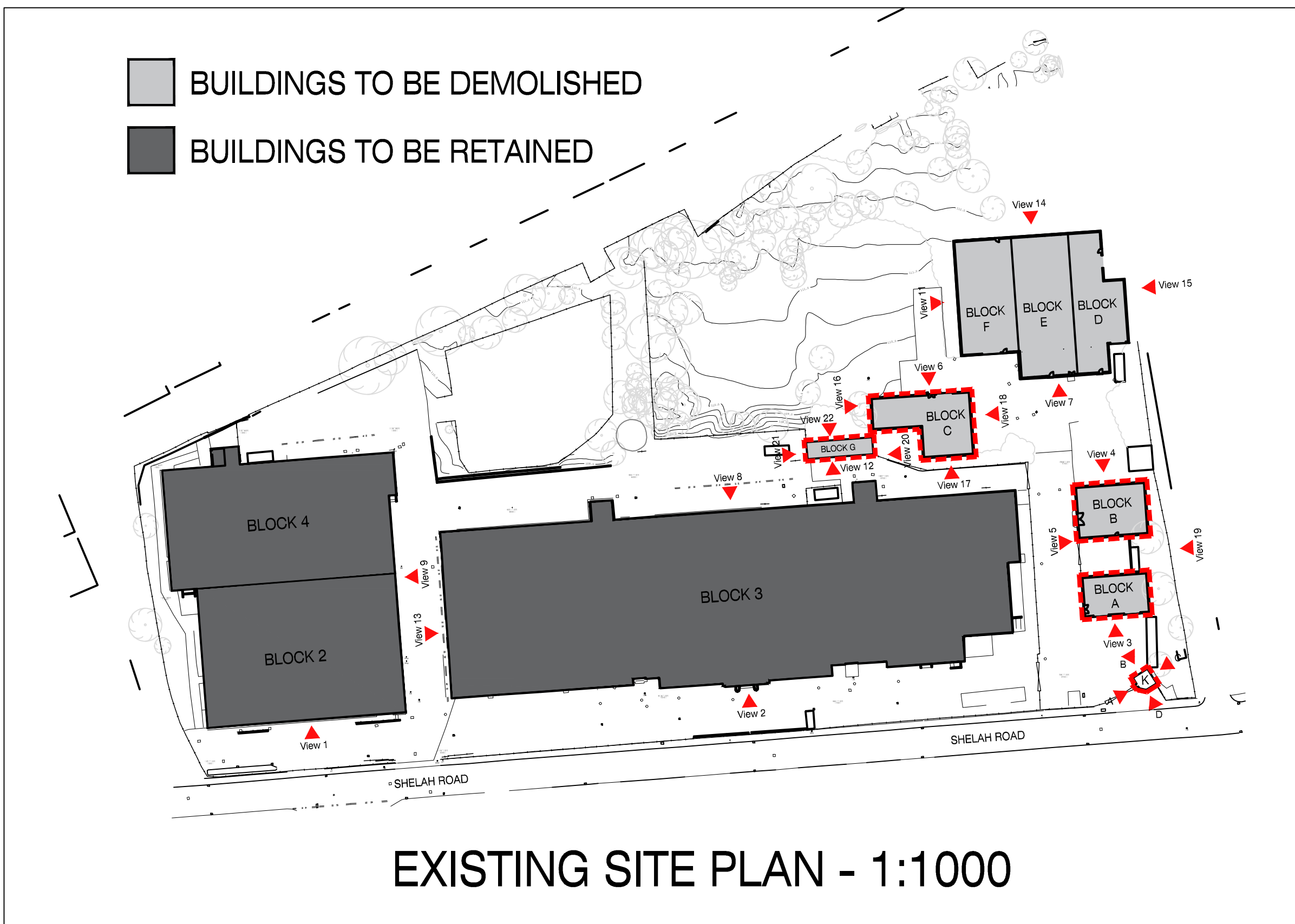
BLOCK G - FRONT
ELEVATION - 1:200
VIEW 22



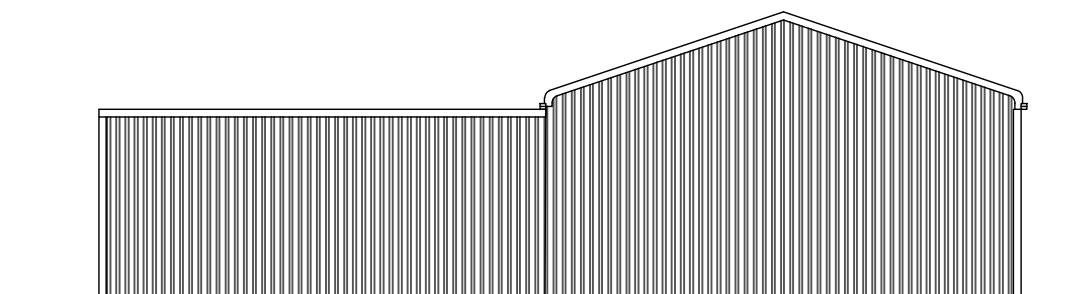
BLOCK G - FRONT
ELEVATION - 1:200
VIEW 21



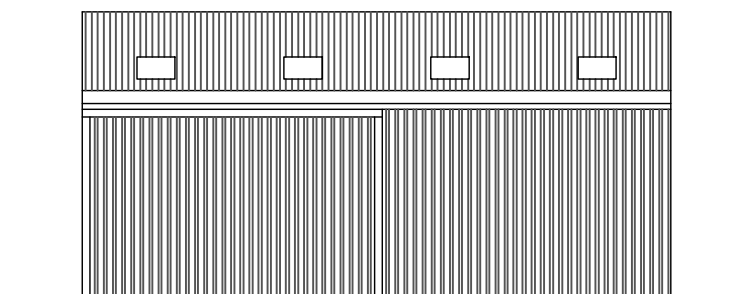
BLOCK G - FRONT
ELEVATION - 1:200
VIEW 20



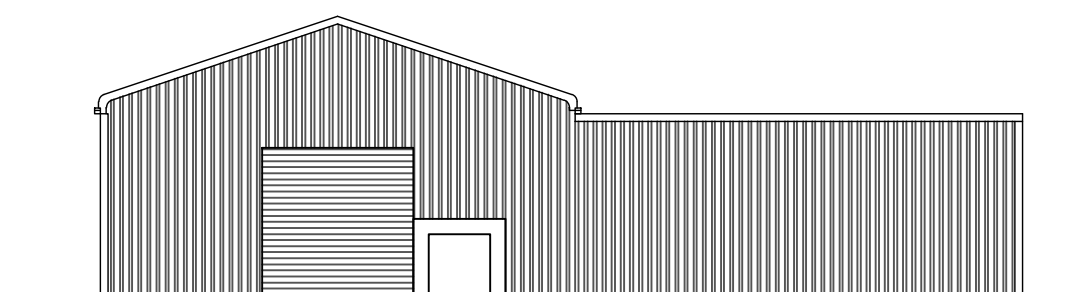
EXISTING SITE PLAN - 1:1000



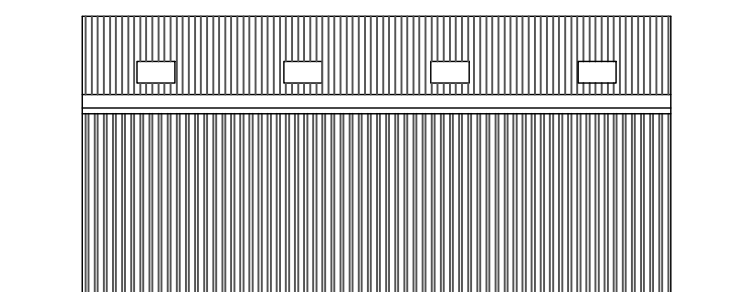
BLOCK C - FRONT
ELEVATION - 1:200
VIEW 17



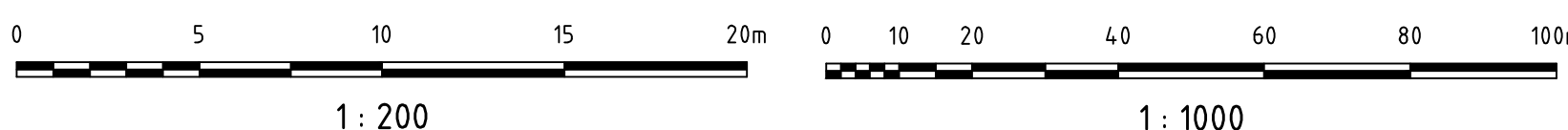
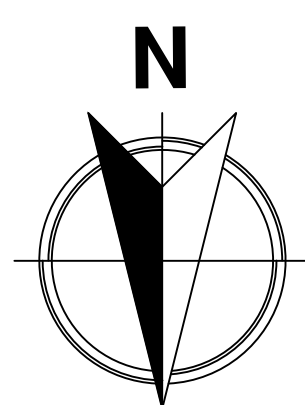
BLOCK C - SIDE
ELEVATION - 1:200
VIEW 16



BLOCK C - REAR
ELEVATION - 1:200
VIEW 6



BLOCK C - SIDE
ELEVATION - 1:200
VIEW 18



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rev. date | drawn | chk'd |

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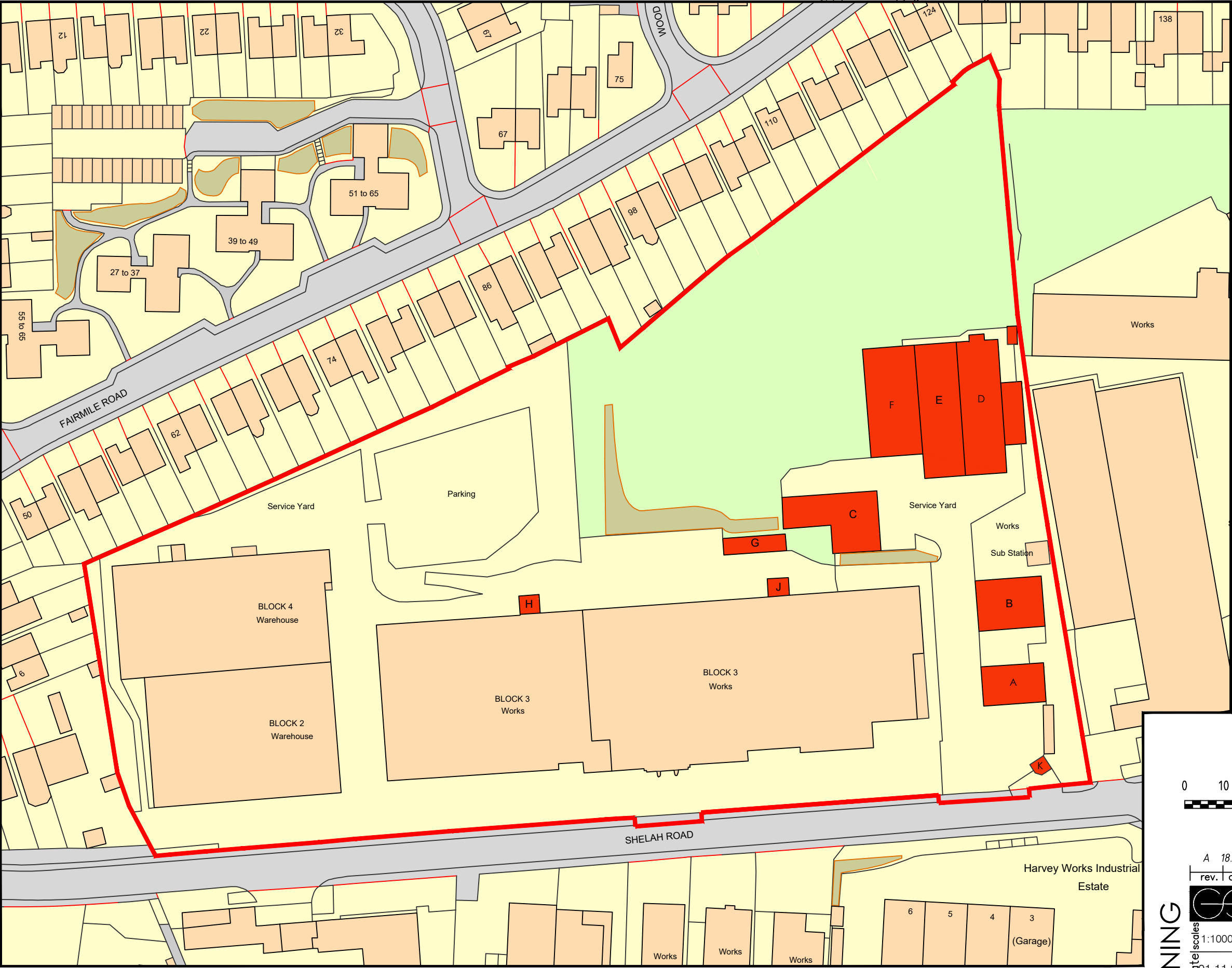
Hayley Group Limited
Shelah Road
Halesowen
West Midlands

Blocks A, B, C, G & K Plan & Elevations As Existing
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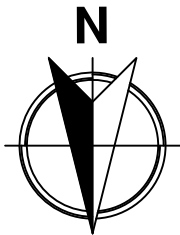
number drawn PS 21-1883-008A
date 17.11.2022
scale Varies@A1



BUILDINGS TO BE DEMOLISHED

BLOCK A	158.0 m ²
BLOCK B	217.0 m ²
BLOCK C	282.9 m ²
BLOCK D	440.2 m ²
BLOCK E	362.6 m ²
BLOCK F	381.6 m ²
BLOCK G	69.8 m ²
BLOCK H	24.9 m ²
BLOCK J	24.7 m ²
BLOCK K	15.5 m ²

TOTAL 1,977.2 m²



1 : 1000

A	18.11.22	General Planning Application Updated	PS	SC
rev.	date		drawn	chk'd



Gould Singleton Architects
Earls Way, Halesowen, West Midlands, B63 3HR

Tel: 0121 550 0359 Fax: 0121 550 8088
e-mail: info@gould-singleton.co.uk

1:1000@A3	
01.11.2022	
PS	SC
checked	
21-1883-010A	

Hayley Group Limited
Shelah Road
Halesowen
West Midlands - B63 3XL

Block Plan - Demolitions

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SITE PLAN - DEMOLITIONS Scale - 1:1000

PLANNING

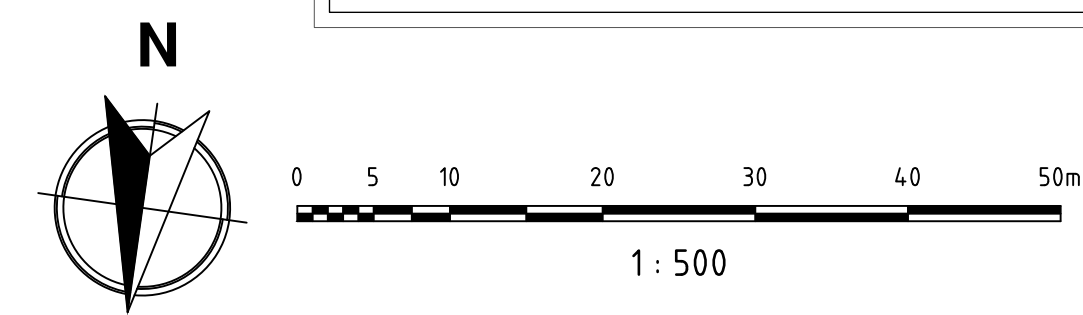


LEGEND

- Existing - Surveyed Trees
- Existing - Trees & shrubs: To be removed
- Proposed - New Trees: Refer to landscape proposals
- Compactor: Existing unit to be re-located
- PV Solar Panels: Proposed shown as indicative

PARKING

- 14.0 Tonne - Total: 7no. parking spaces
- 7.5 Tonne - Total: 6no. parking spaces
- Vans - Total: 15no. parking spaces
- Existing car parking spaces: 181 - incl 1no. disabled
- New 2.5m x 5.3m parking spaces: 141 - incl 3no. disabled
- TOTAL CAR PARKING SPACES** 220 - incl 4no. disabled (existing retained & proposed)
- EV charging points - 7Kw 6no. dual posts: 12 spaces total
- 2no. bicycle stores: total 28no. bicycles
- Commercial vehicle parking zones



SITE PLAN AS PROPOSED SCALE 1:500

C	12.04.23	acoustic fence height increased	PS	SC
B	16.02.23	Parking layout amended	PS	SC
A	18.11.22	General Planning Application Updated	PS	SC
rev.	date		drawn	chk'd

PLANNING

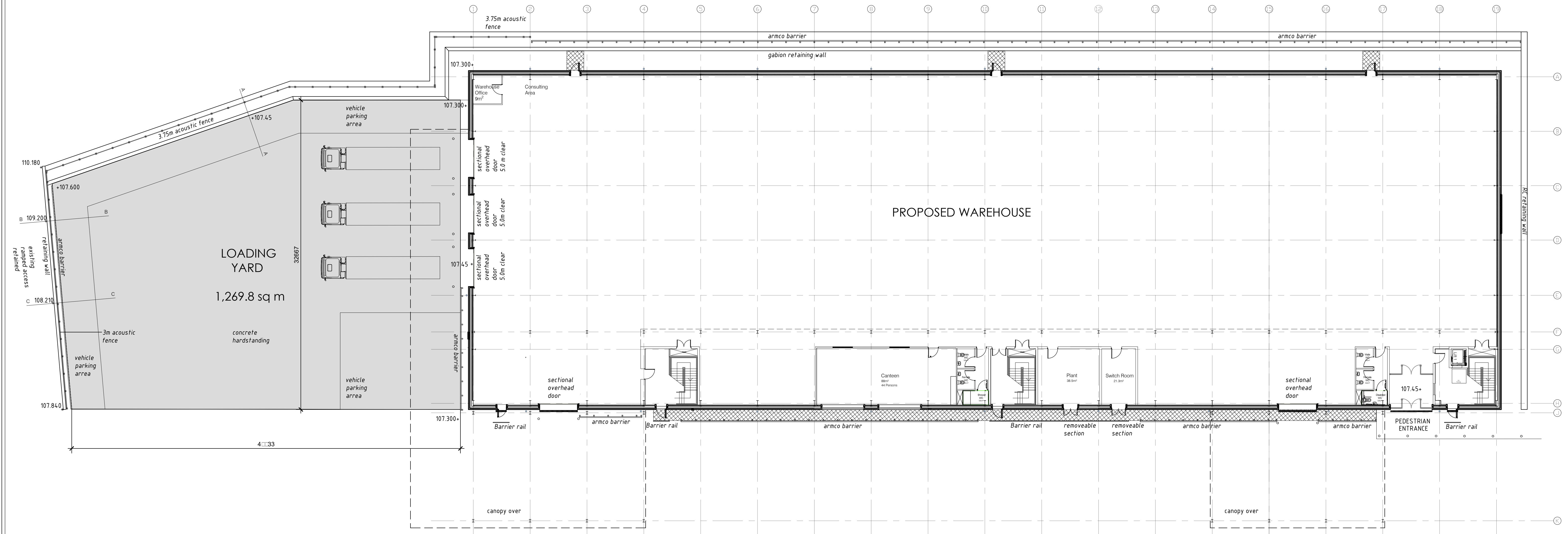
Gould Singleton Architects
Earls Way, Halesowen, West Midlands, B63 3HR
Tel: 0121 550 0359 Fax: 0121 550 8088
e-mail: info@gould-singleton.co.uk

Hayley Group Limited
Shelah Road
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B63 3XL

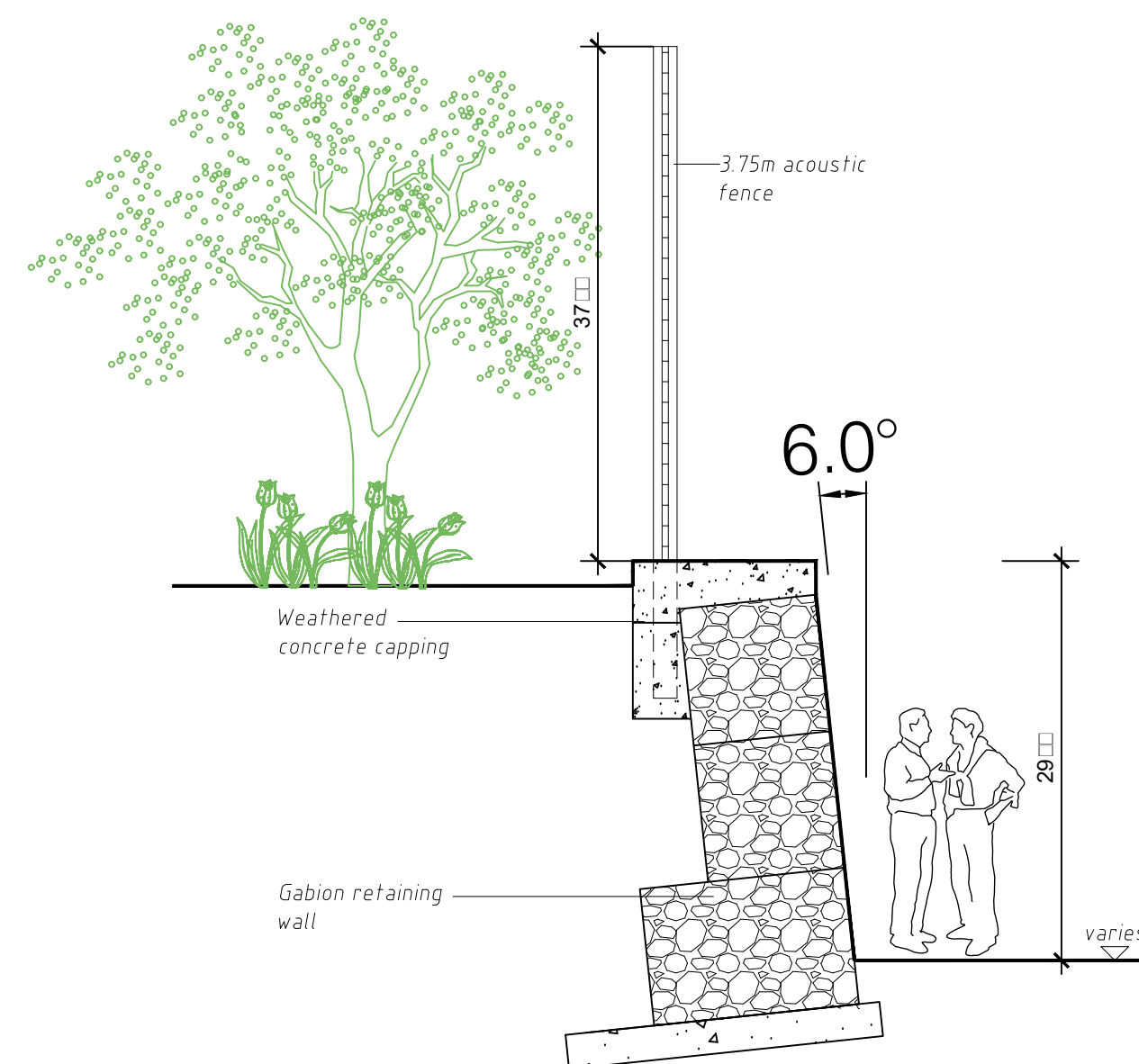
Overall Site Plan As Proposed

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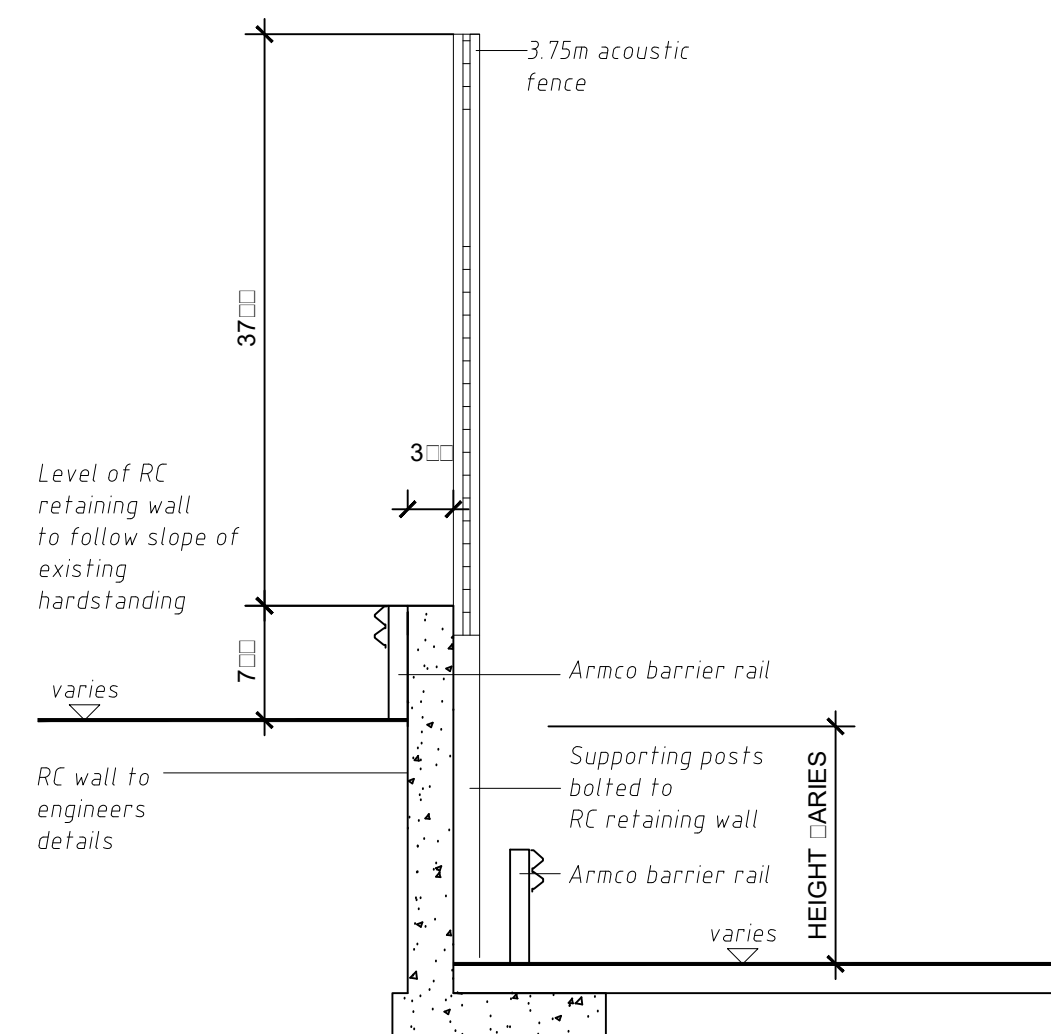
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date 01.11.2022
scales 1:500 @ A1
21-1883-011C



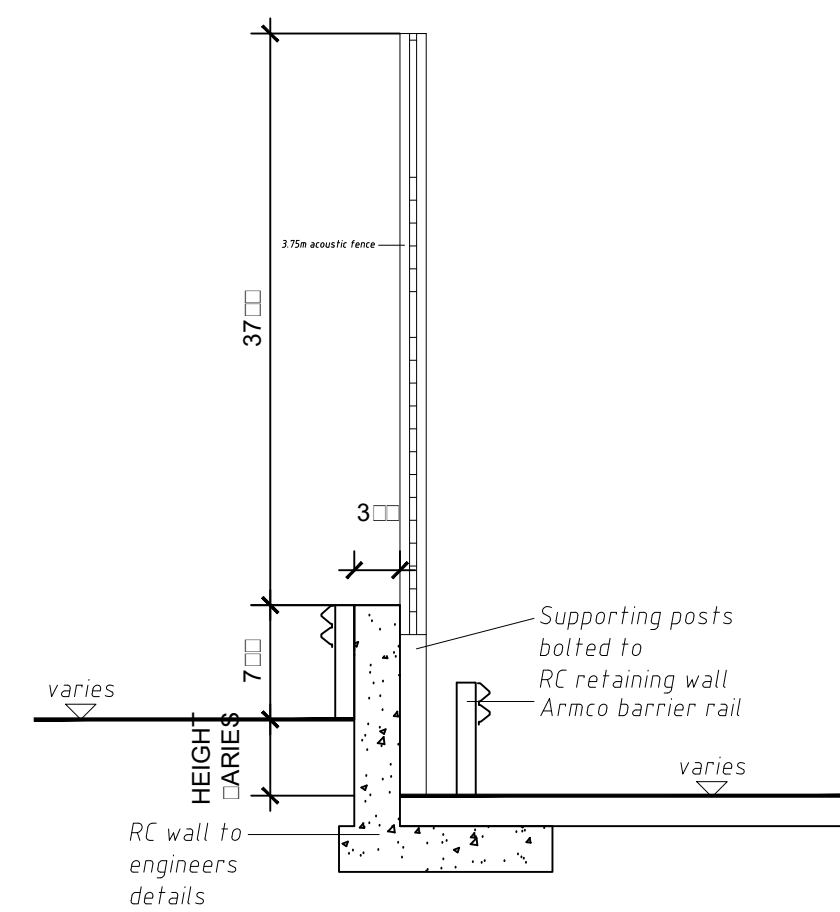
GROUND FLOOR PLAN SCALE 1:200
GROSS INTERNAL AREA 3,729 sqm



Section AA



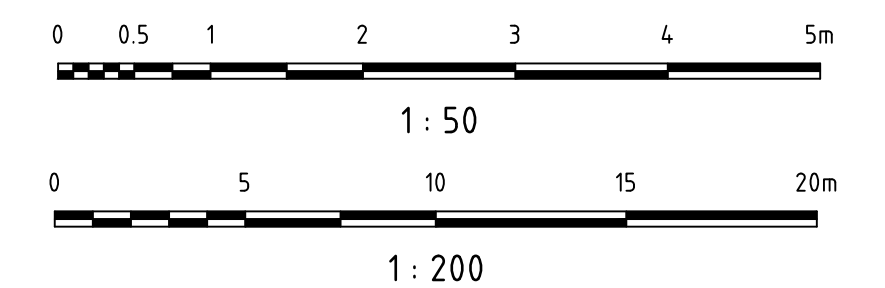
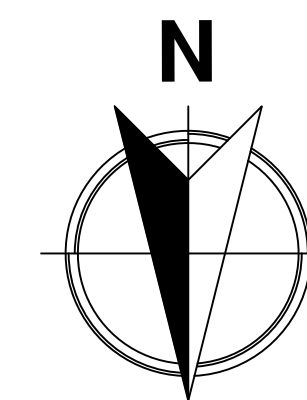
Section BB



Section CC

TYPICAL WALL SECTIONS

SCALE 1:50



rev.	date	description	PS	SC
B	12.04.23	acoustic fence height increased	PS	SC
A	18.11.22	General Planning Application Updated	PS	SC

drawn | checked |
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number
drawn
date
1:200,50 @ A1
01.11.2022

checked
CN PS
21-1883-012B

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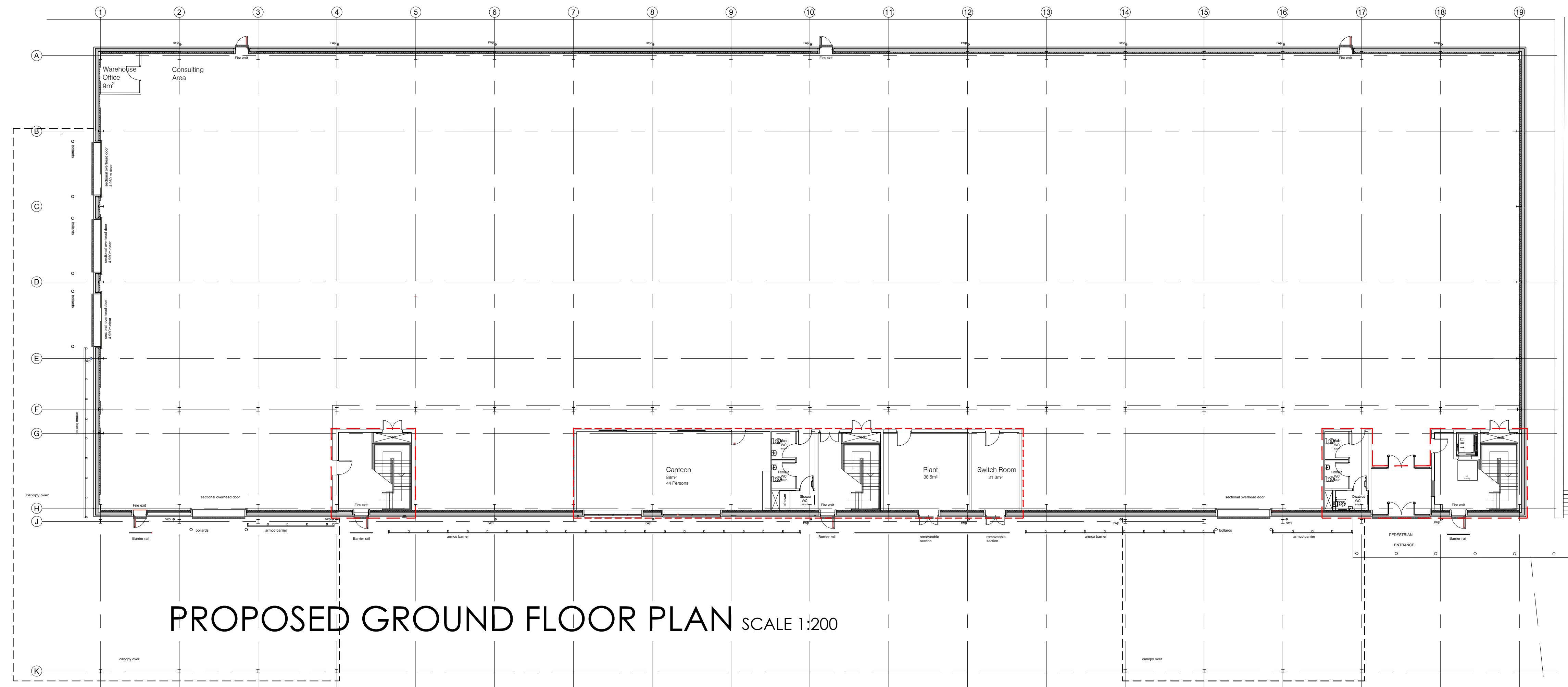
Proposed Warehouse Ground Floor Plan & Gabion Sections

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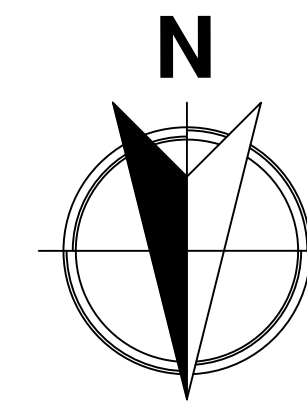
PLANNING



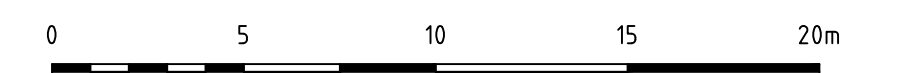
PROPOSED FIRST FLOOR PLAN SCALE 1:200



PROPOSED GROUND FLOOR PLAN SCALE 1:200



GROUND FLOOR AREA OF ANCILLARY = 371.5 sqm
FIRST FLOOR AREA OF ANCILLARY = 790.8 sqm
TOTAL BUILDING INTERNAL AREA = 3,729 sqm



1 : 200

A 18.11.22

General Planning Application Updated

PS SC

rev. date

drawn/chk'd



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PLANNING

number drawn date scales

1:200@A1

01.11.2022

PS SC

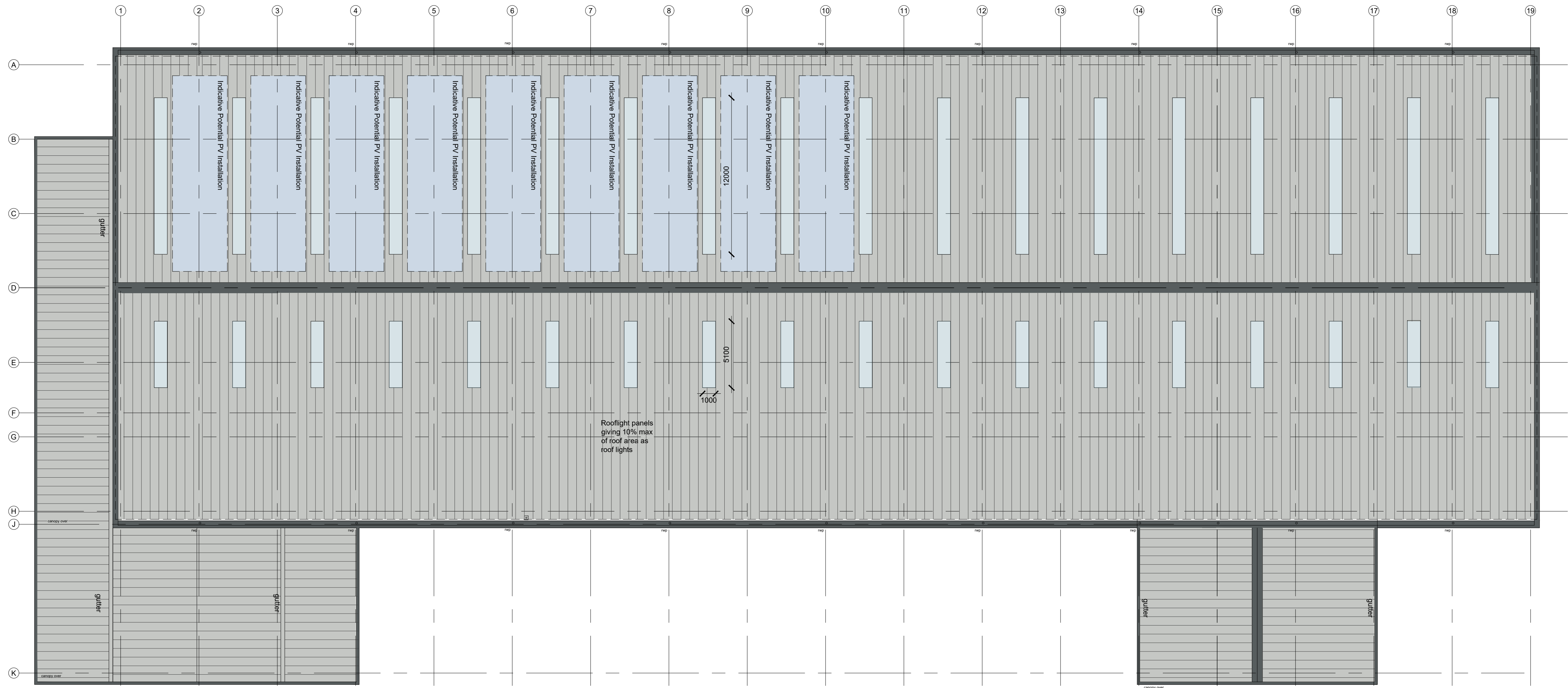
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21-1883-013A

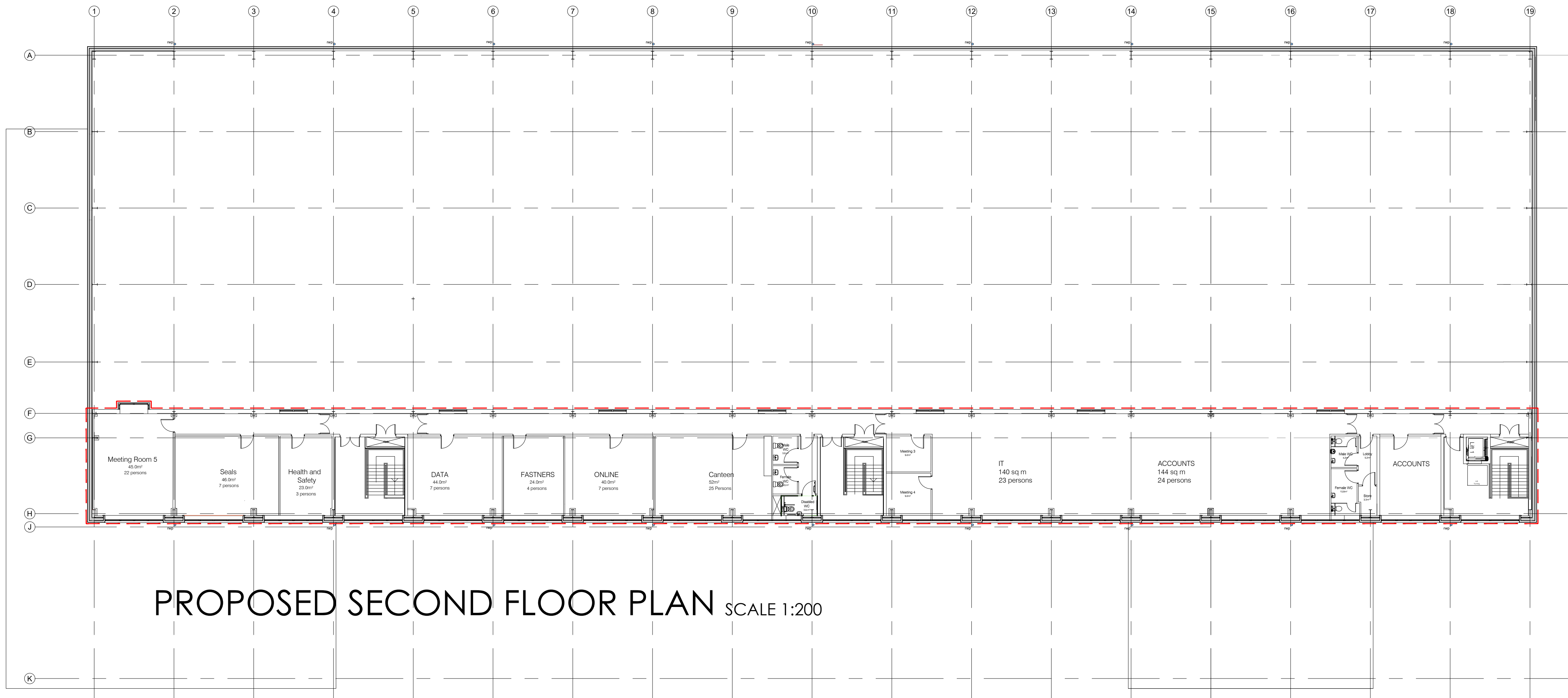
Hayley Group Limited
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West Midlands

New Warehouse Ground & First Floor Plans

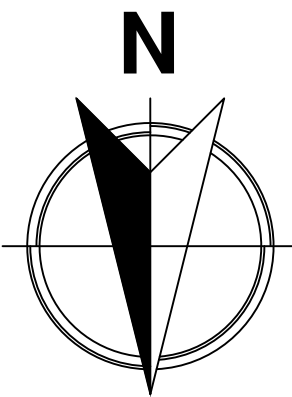
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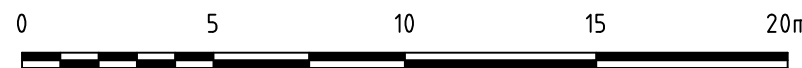
PROPOSED ROOF PLAN SCALE 1:200



PROPOSED SECOND FLOOR PLAN SCALE 1:200



SECOND FLOOR AREA OF ANCILLARY = 950sqm
TOTAL BUILDING INTERNAL AREA = 3,729 sqm



1 : 200

A 18.11.22

General Planning Application Updated

PS SC

rev. | date

drawn | chk'd



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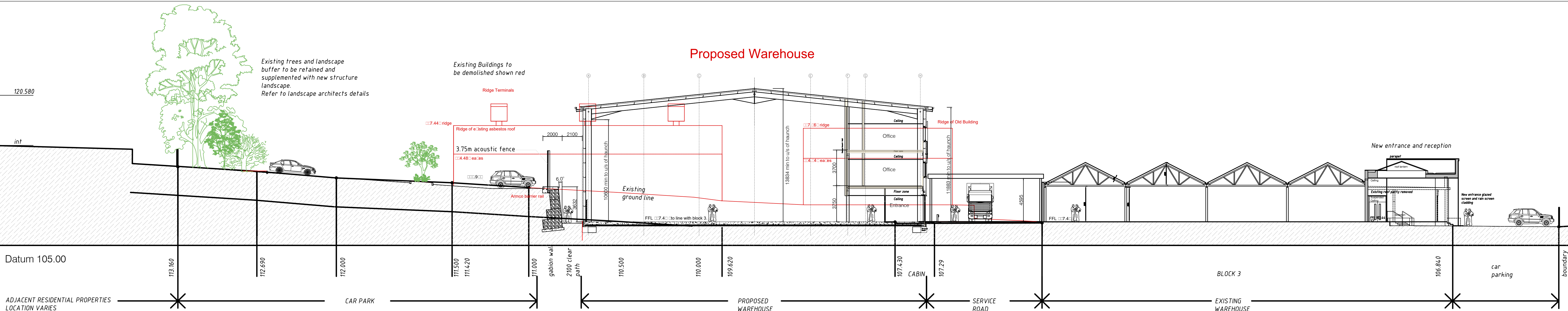
Hayley Group Limited
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West Midlands

New Warehouse Second Floor Plan & Roof Plan

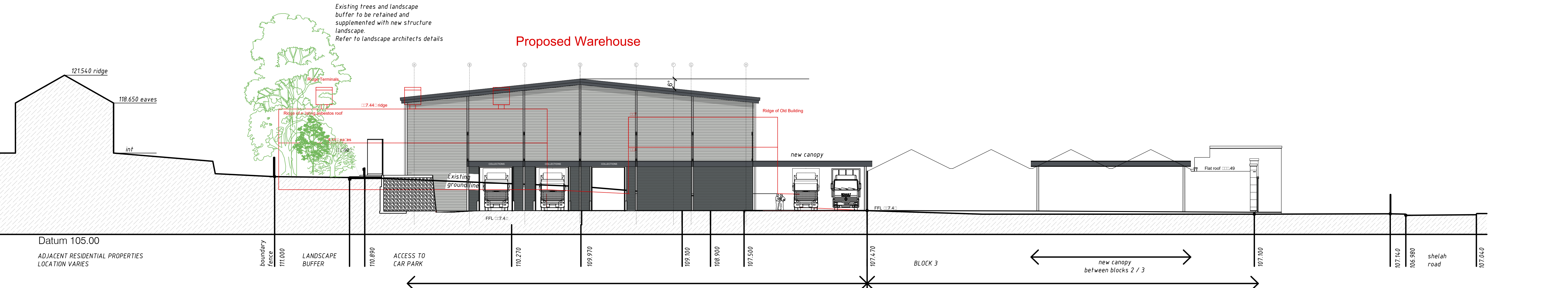
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PLANNING

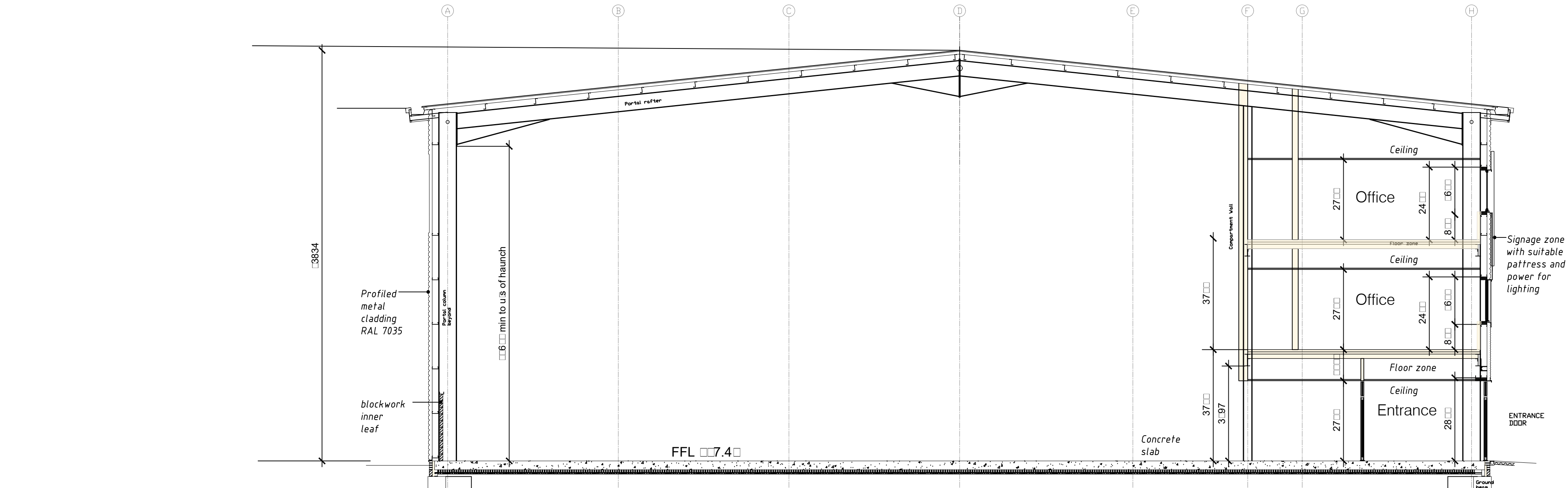
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drawn | 1:200@A1 | 01.11.2022 |
PS | SC |
21-1883-014A



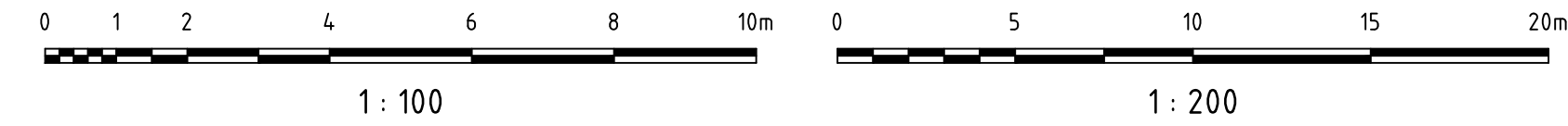
PROPOSED SECTION A-A SCALE 1:200



PROPOSED SECTION B-B SCALE 1:200



PROPOSED SECTION C-C SCALE 1:100



B	12.04.23	acoustic fence increased to 3.75m	PS	SC
A	18.11.22	General Planning Application Updated	PS	SC
rev.	date		drawn	chk'd

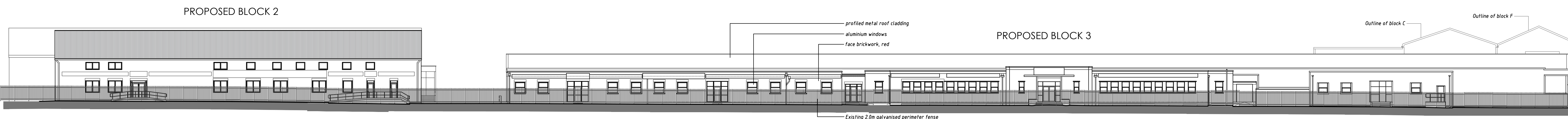
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West Midlands - B63 3XL

New Warehouse Site Sections As Proposed

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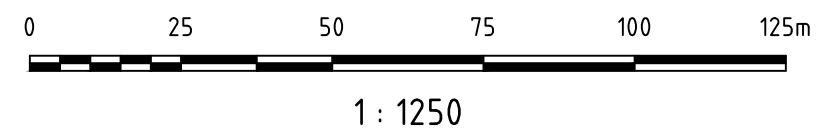
PLANNING



EXISTING EXTERNAL ELEVATION FROM SHELAH ROAD
SCALE 1:250 @ A1



PROPOSED EXTERNAL ELEVATION FROM SHELAH ROAD
SCALE 1:250 @ A1



PLANNING	number	A	18.11.22	General Planning Application Updated		PS	SC
	drawn	rev.	date			drawn	chk'd
	drawn	rev.	date			drawn	chk'd
	drawn	rev.	date			drawn	chk'd
	drawn	rev.	date			drawn	chk'd

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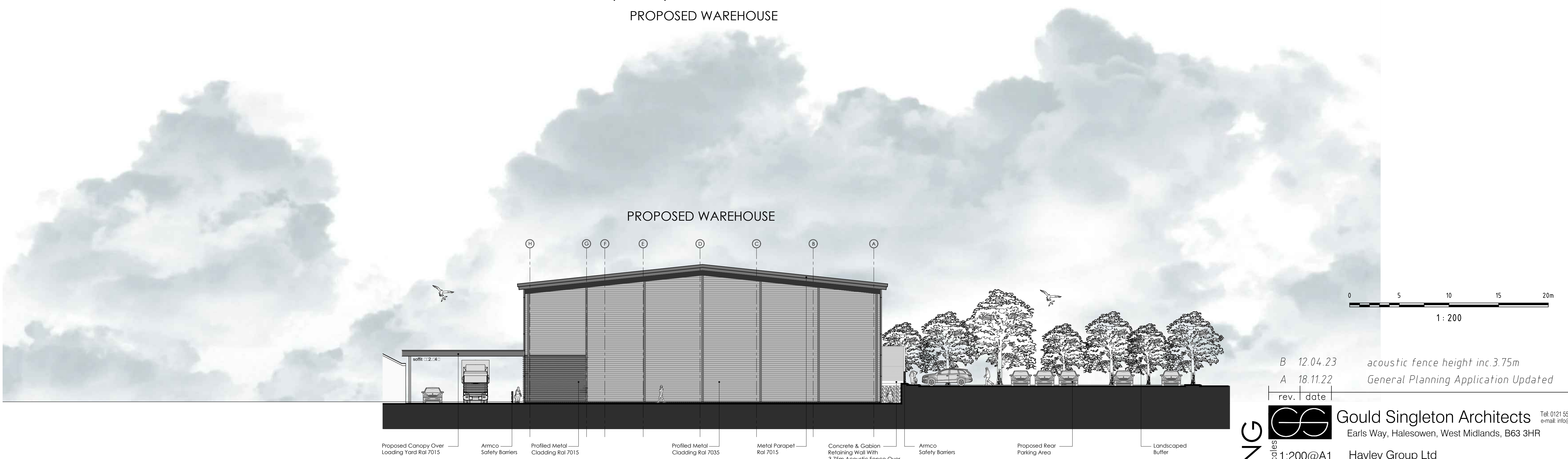
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Proposed Street Scene North - South Elevation

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PROPOSED EXTERNAL (EAST)ELEVATION FROM LOADING YARD
PROPOSED WAREHOUSE



PROPOSED EXTERNAL (WEST)ELEVATION

PLANNING

number	21-1883-017B
drawn	CN
checked	PS
date	01.11.2022
scales	1:200@A1

rev.	date	description	drawn	chk'd
B	12.04.23	acoustic fence height inc.3.75m	PS	SC
A	18.11.22	General Planning Application Updated	PS	SC

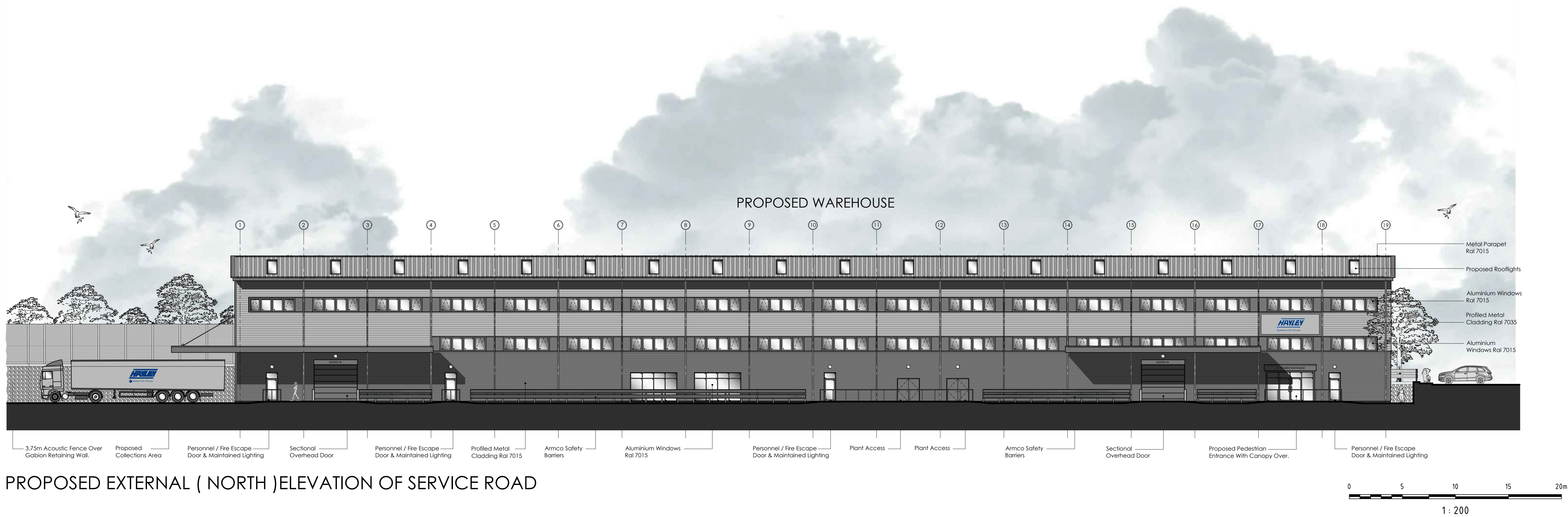


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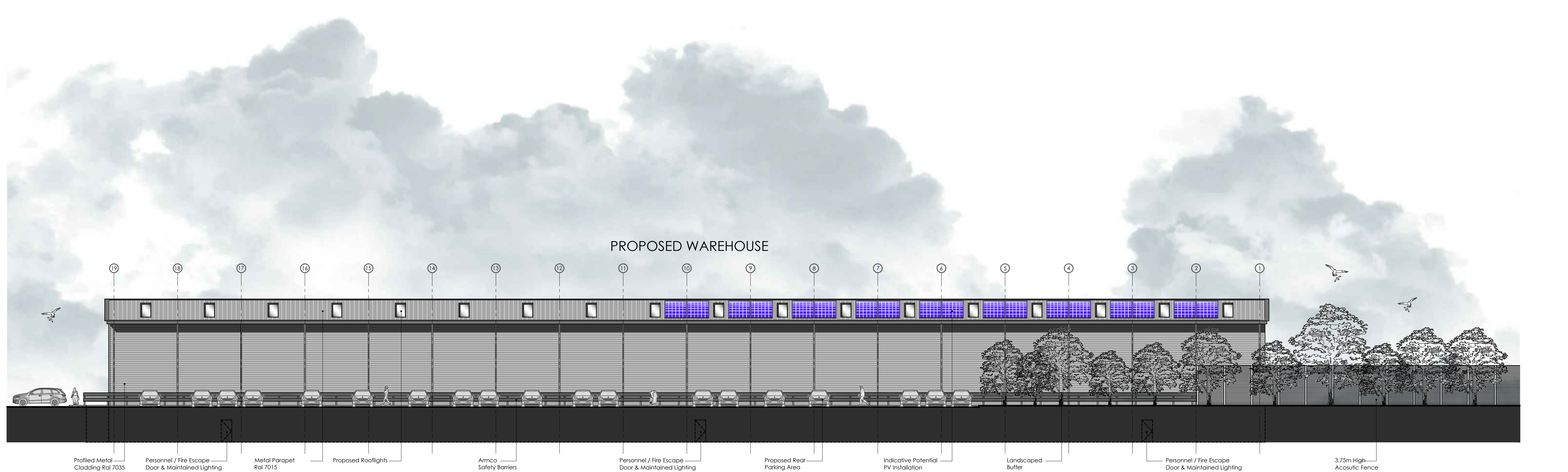
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Proposed Warehouse East & West Side Elevations

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PROPOSED EXTERNAL (NORTH)ELEVATION OF SERVICE ROAD



PROPOSED EXTERNAL REAR(SOUTH) ELEVATION FROM CARPARK

rev.	date	description	PS	SC
B	12.14.23	acoustic fence height inc. to 3.75m	PS	SC
A	18.11.22	General Planning Application Updated	PS	SC

drawn: chkd

PLANNING

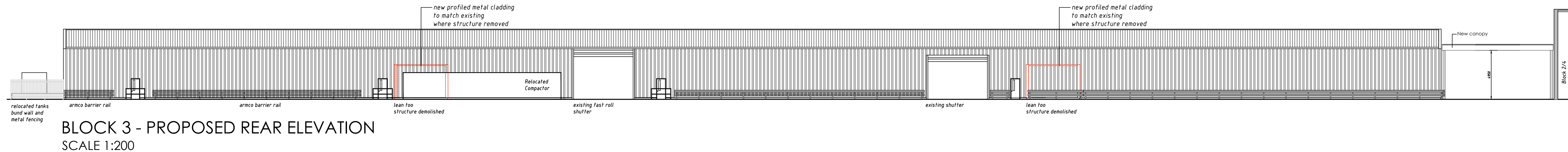
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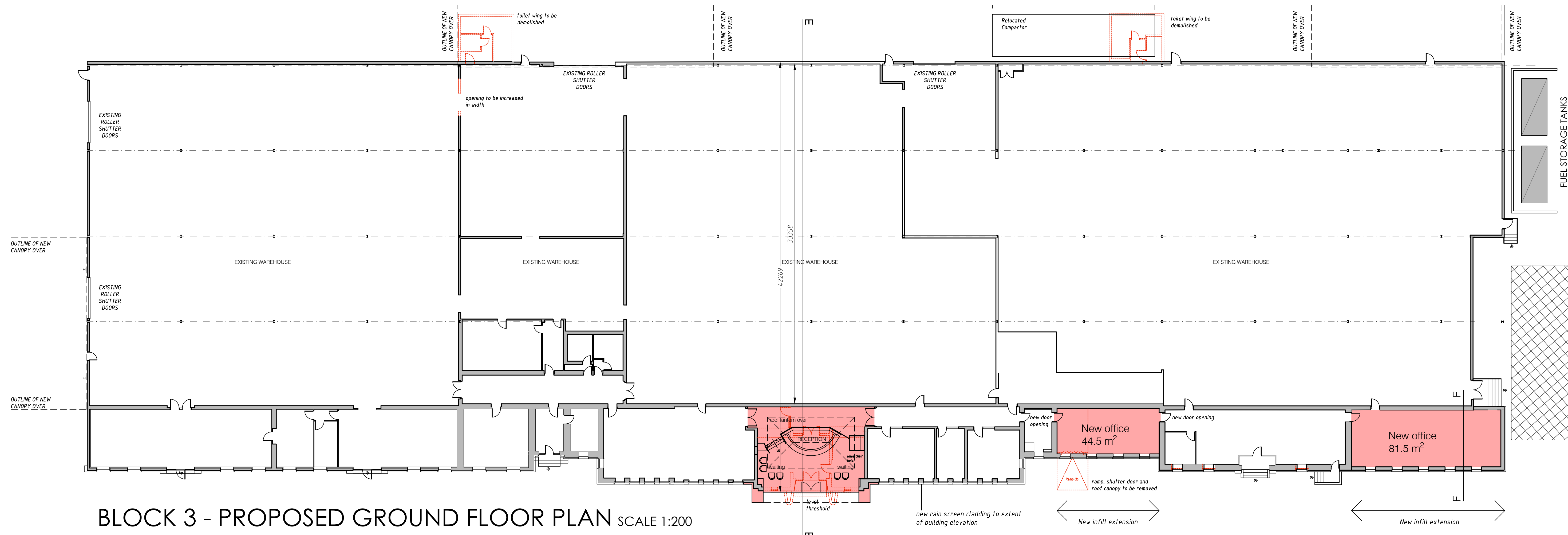
Proposed Warehouse North - South Front & Rear Elevs

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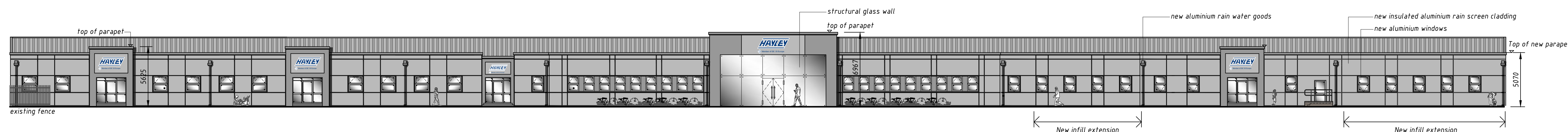
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number: -1883-018B



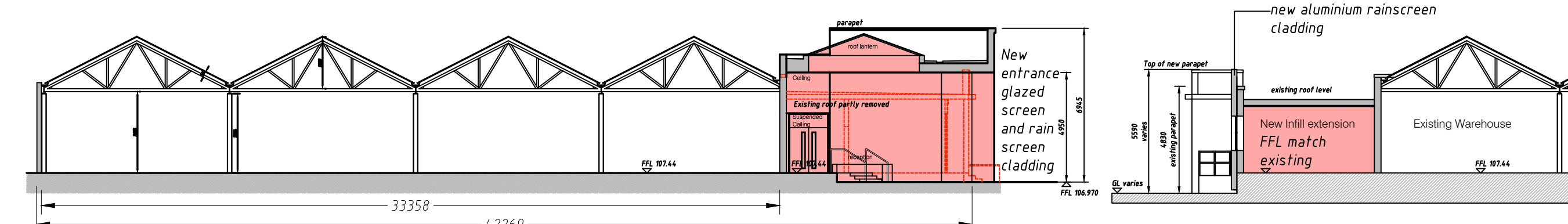
BLOCK 3 - PROPOSED REAR ELEVATION
SCALE 1:200



BLOCK 3 - PROPOSED GROUND FLOOR PLAN SCALE 1:200

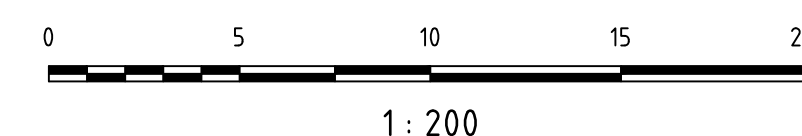


BLOCK 3 - PROPOSED ELEVATION FROM SHELAH ROAD SCALE 1:200



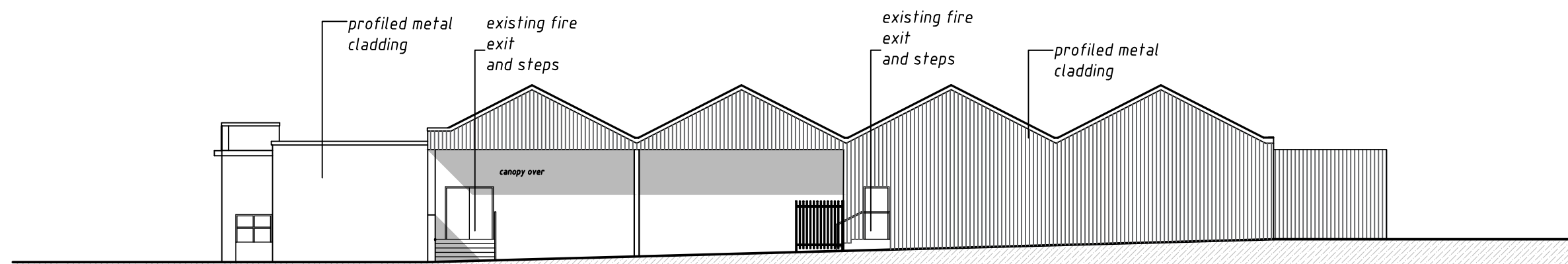
BLOCK 3 - PROPOSED SECTION E-E
SCALE 1:200

BLOCK 3 - PROPOSED SECTION F-F
SCALE 1:200



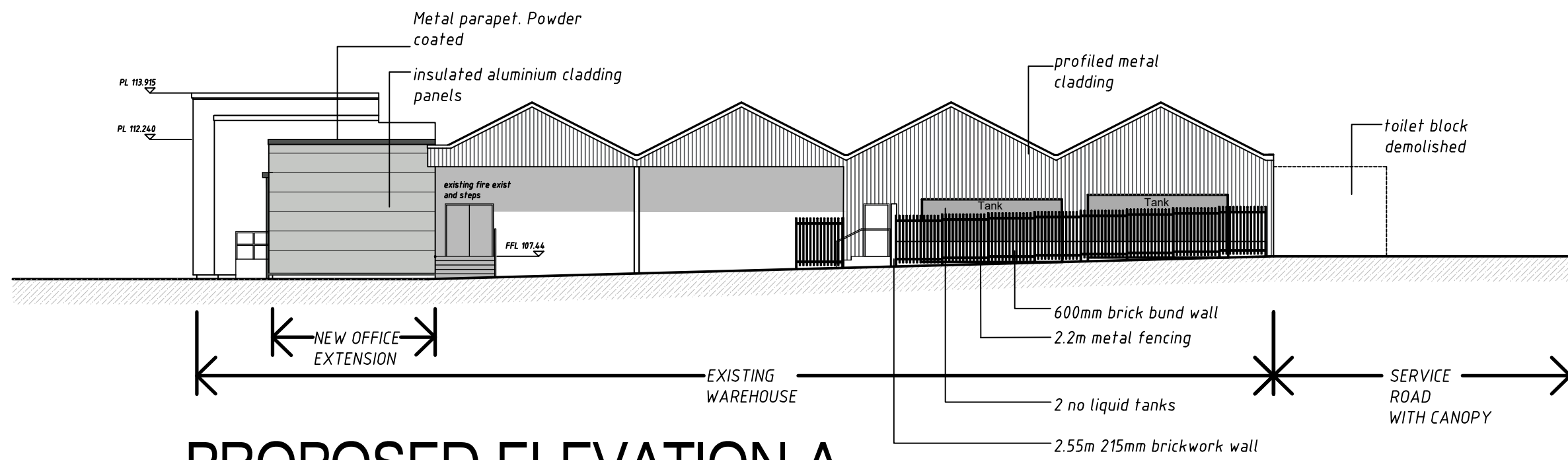
number	drawn	date	18.11.22	rev.	date	18.11.22	General Planning Application Updated	PS	SC	drawn	chk'd
Gould Singleton Architects Earls Way, Halesowen, West Midlands, B63 3HR Hayley Group Limited Shelah Road Halesowen West Midlands											
Block 3 - Proposed Alterations											
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PLANNING
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date 01.11.2022
checked PS SC
1-1883-019A



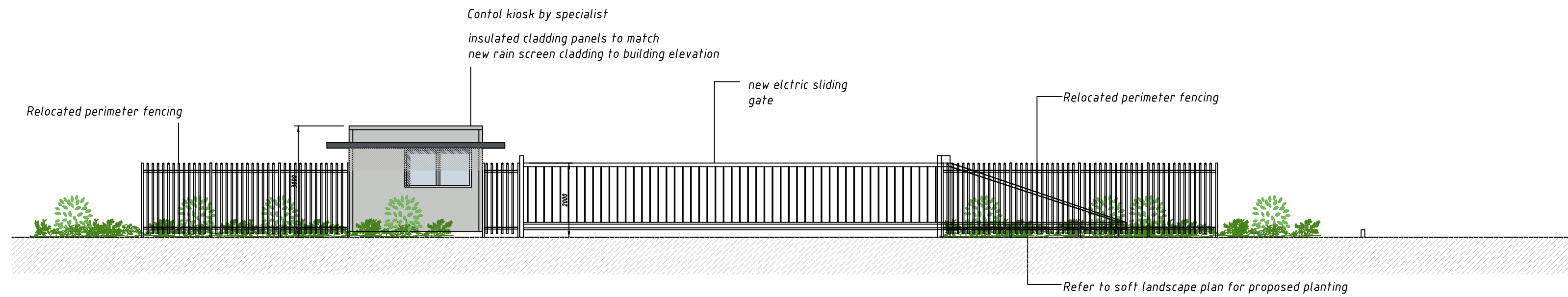
EXISTING ELEVATION A

SCALE 1:200



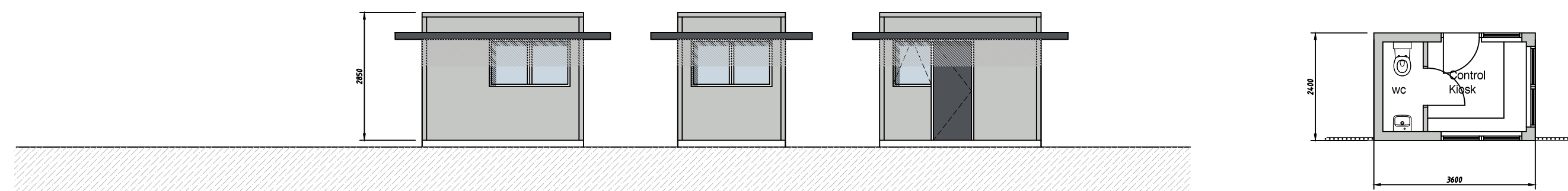
PROPOSED ELEVATION A

SCALE 1:200



KIOSK STREET ELEVATION B

SCALE 1:100

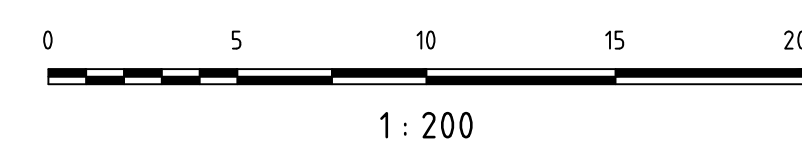
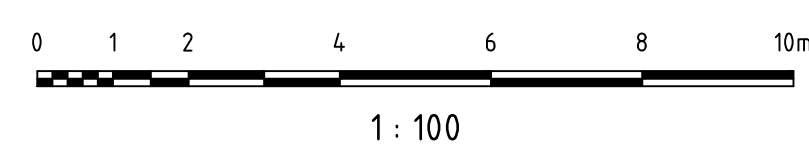


KIOSK ELEVATIONS

SCALE 1:100

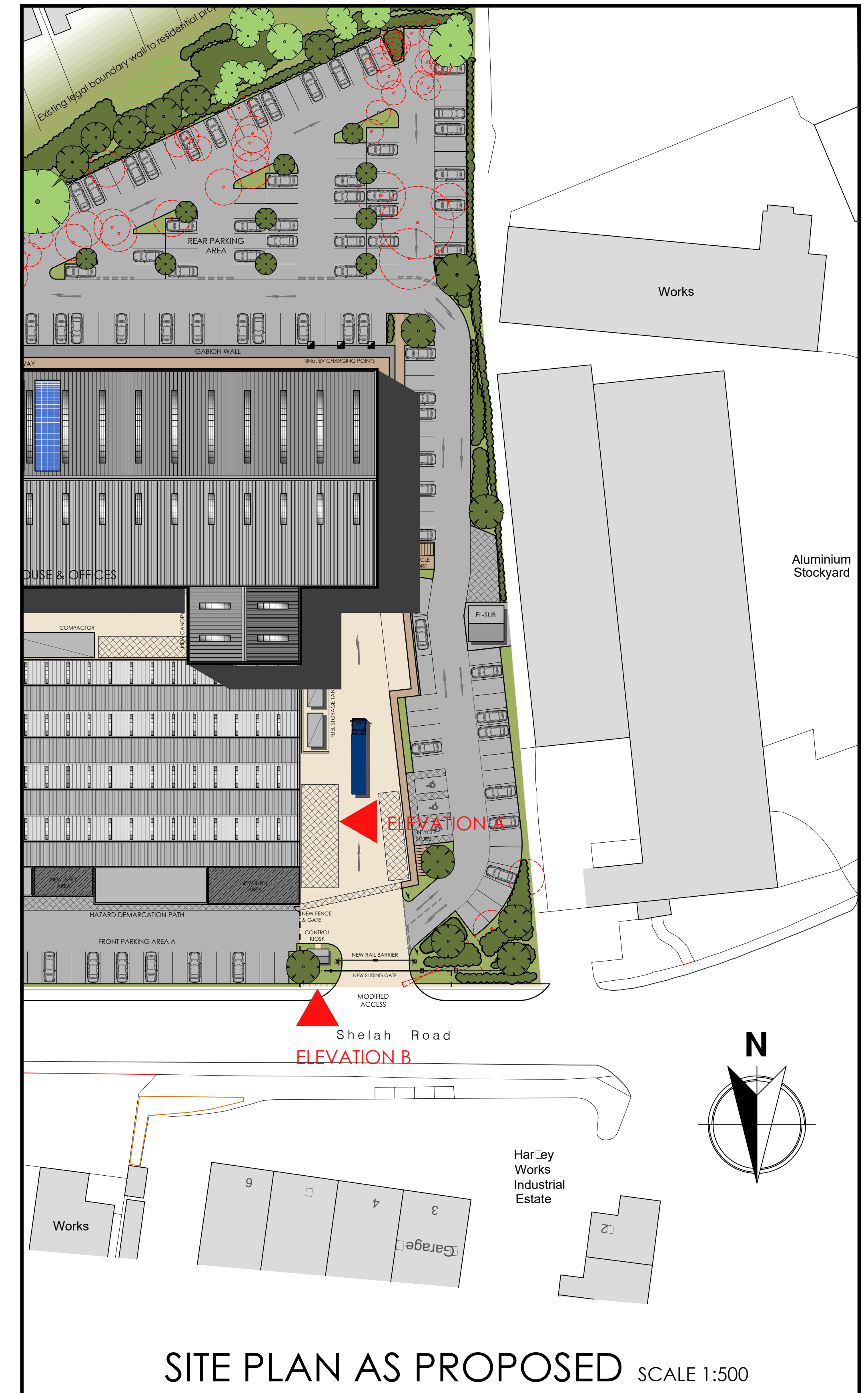
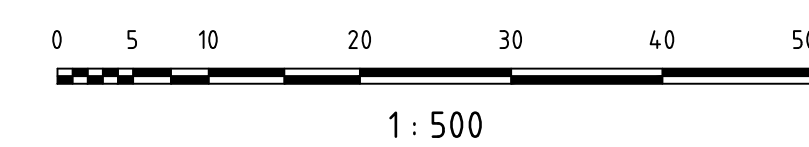
PLAN

SCALE 1:100



Note: Topo Drawings provided from 3rd party. Full survey and dimension check required.

FOR INDICATIVE PURPOSES ONLY



SITE PLAN AS PROPOSED

SCALE 1:500

A	18.11.22	General Planning Application Updated	PS	SC
rev.	date		drawn	chk'd

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Block 3 Proposed Alterations

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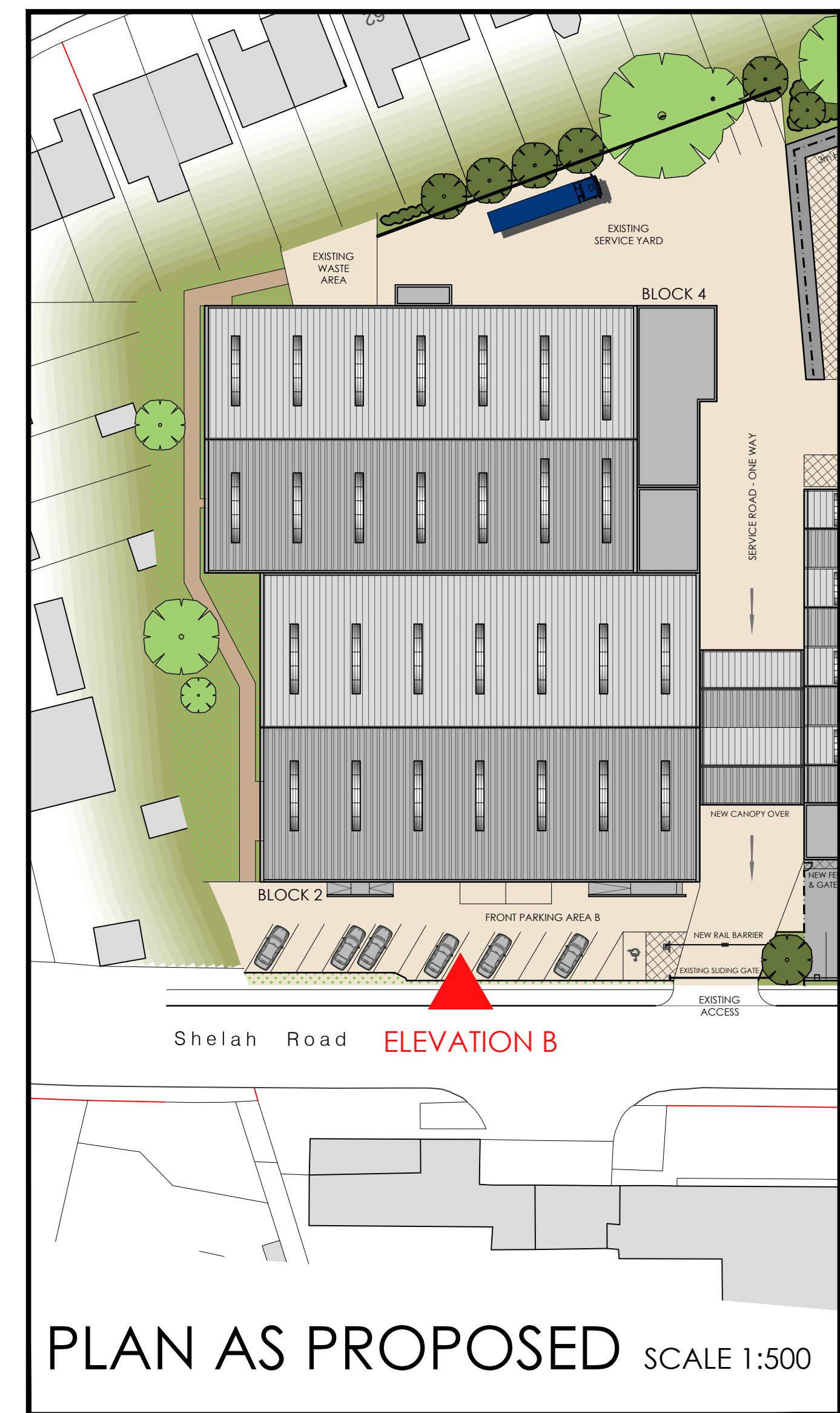
PLANNING

Varies@A1
01.11.2022
PS
1-1883/020A

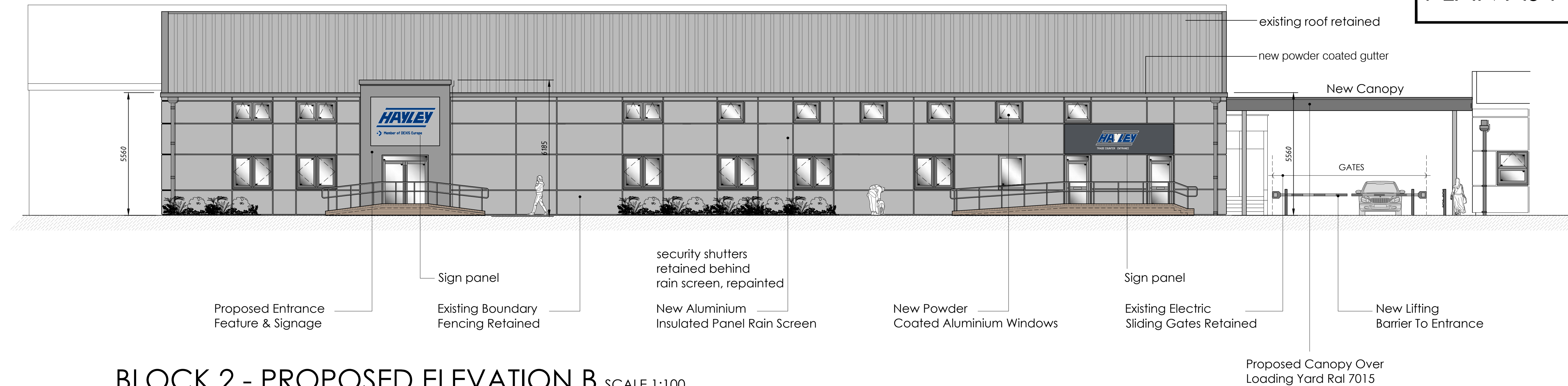
SC



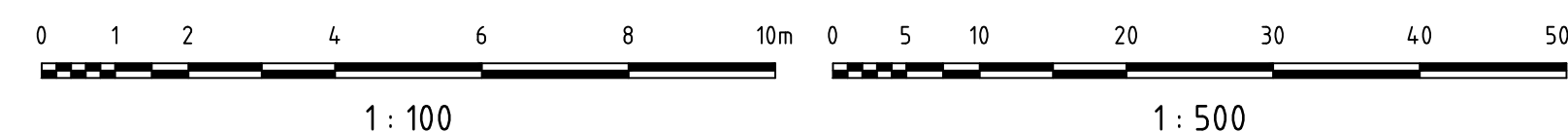
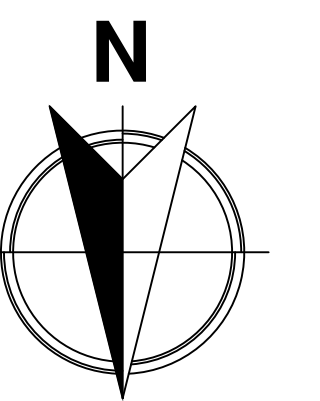
BLOCK 2 - EXISTING ELEVATION B SCALE 1:100



PLAN AS PROPOSED SCALE 1:500



BLOCK 2 - PROPOSED ELEVATION B SCALE 1:100



Note: Topo Drawings provided from 3rd party. Full survey and dimension check required.
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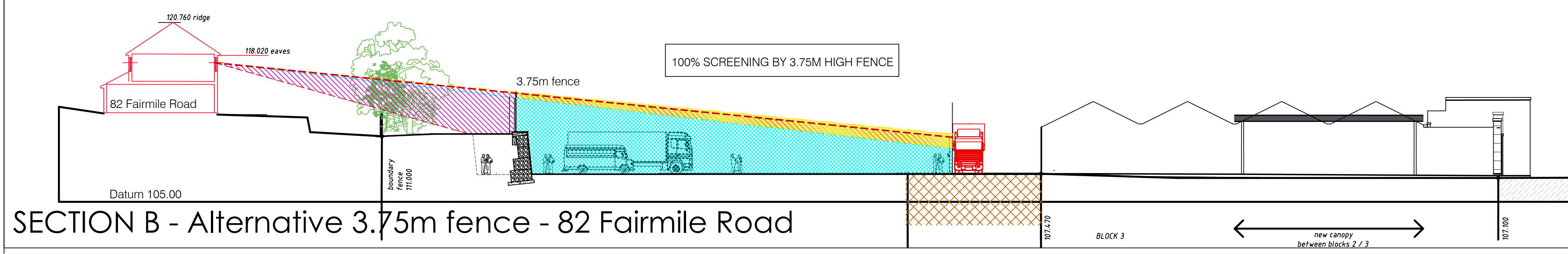
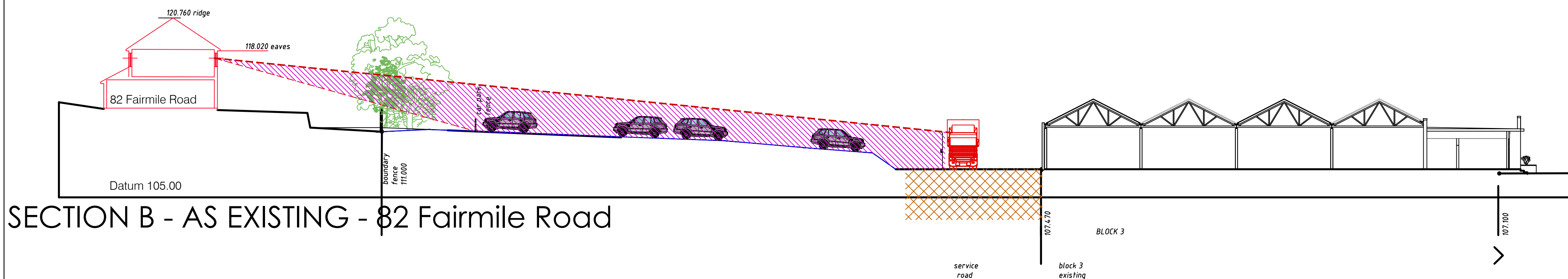
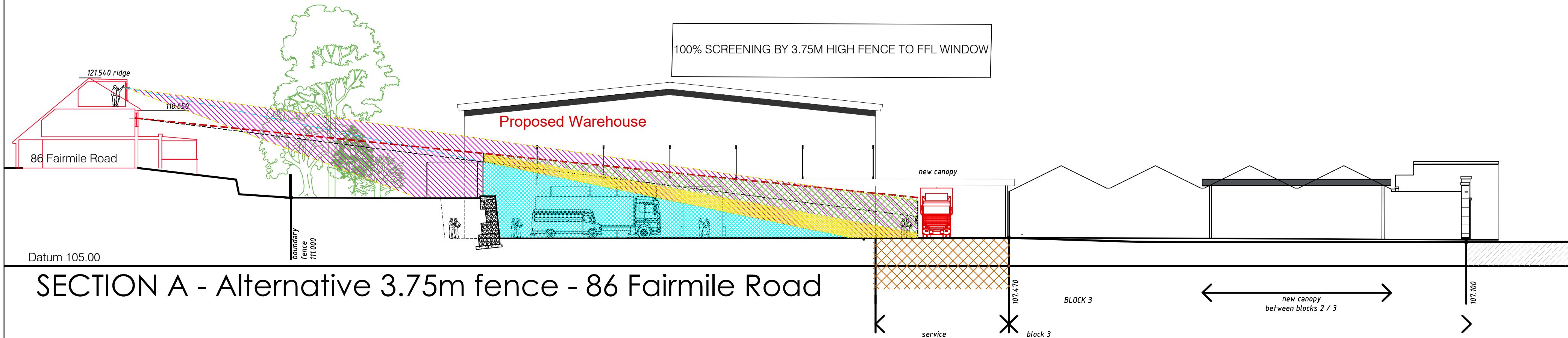
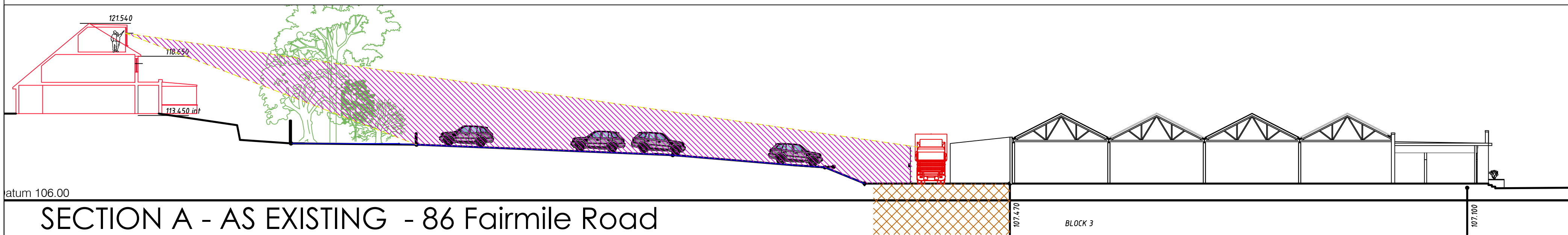
PLANNING

A 18.11.22		General Planning Application Updated		PS	SC
rev. date				drawn	chk'd
		Gould Singleton Architects		Tel: 0121 550 0359 Fax: 0121 550 8088 e-mail: postmaster@gould-singleton.co.uk	
Earls Way, Halesowen, West Midlands, B63 3HR					
number	drawn	checked	PS	SC	
date	01.11.2022	Varies @A1			
		Hayley Group Limited Shelah Road Halesowen West Midlands			
		Block 2 Proposed Alterations			
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21-1883-021A					



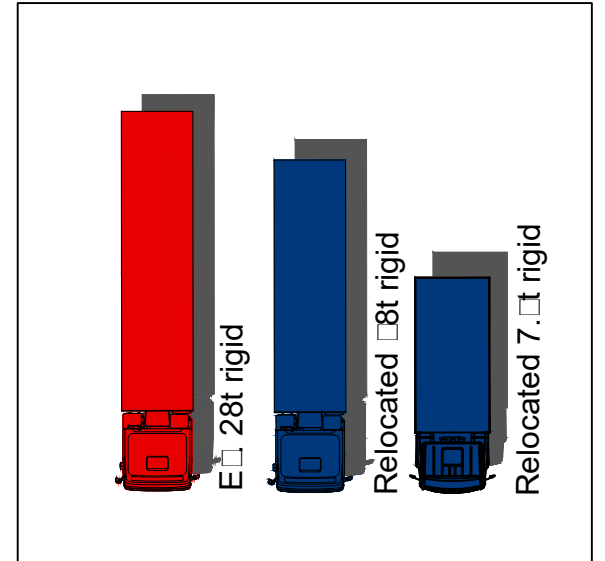
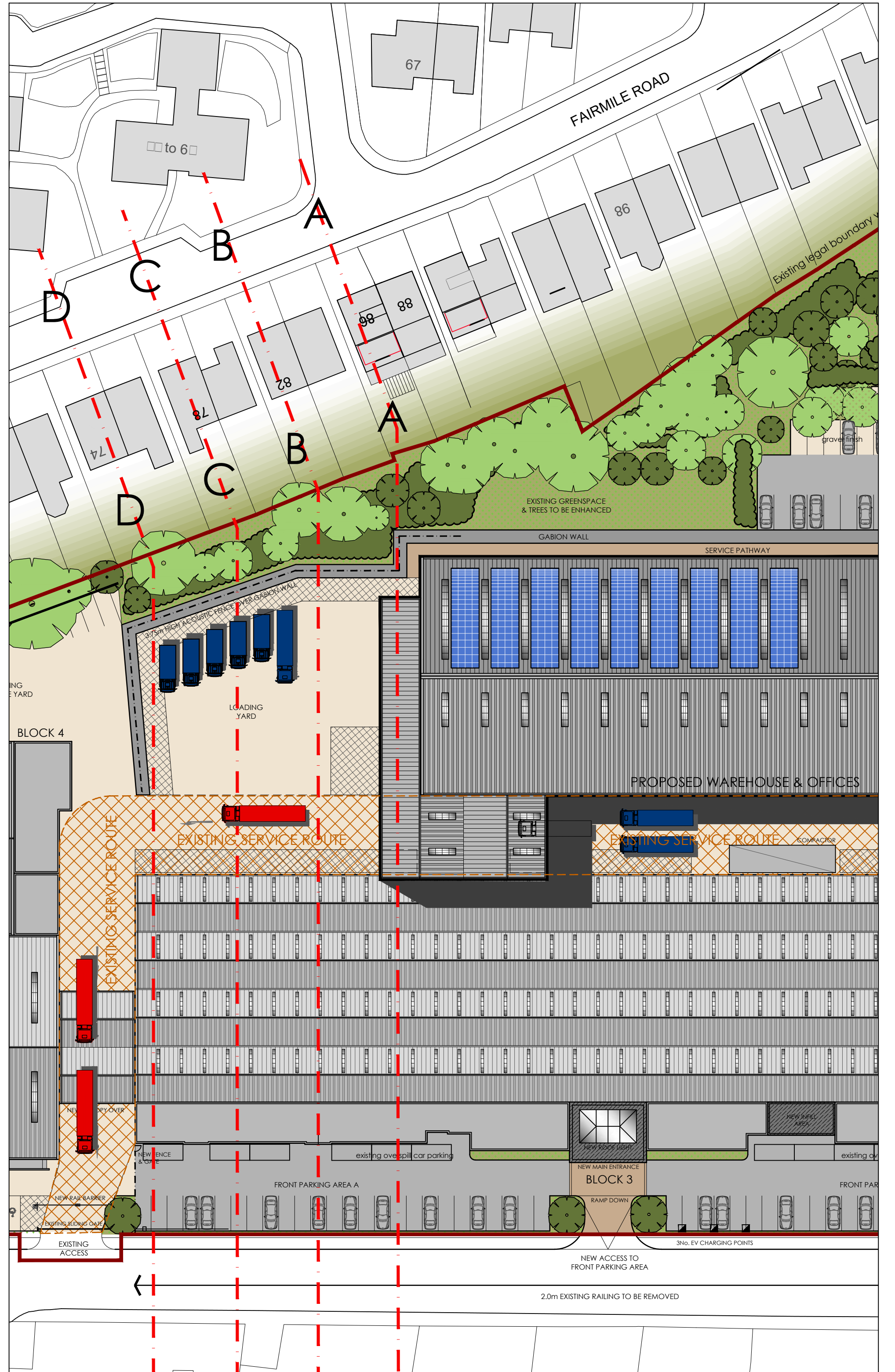
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Tel: 0121 550 0359 Fax: 0121 550 8088
e-mail: postmaster@gould-singleton.co.uk

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Shelah Road
Halesowen
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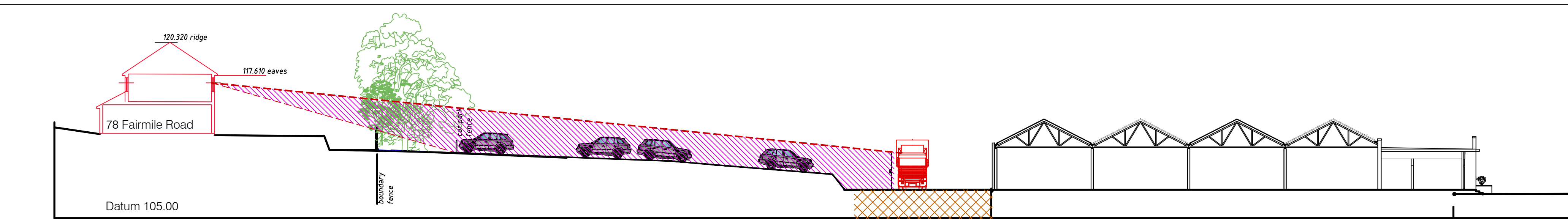


Note
All references to the specification of any product, material or system used in the proposed construction, repair, treatment and refurbishment of any building or structure detailed on this drawing, in respect of the performance of combustibility, fire resistance or fire protection shall be fully in accordance with manufacturer's or supplier's specifications and recommendations and in accordance with current applicable regulations. Where specified refer to fire engineer consultant's specific details where appointed.
No reliance shall be placed on such details on this drawing.

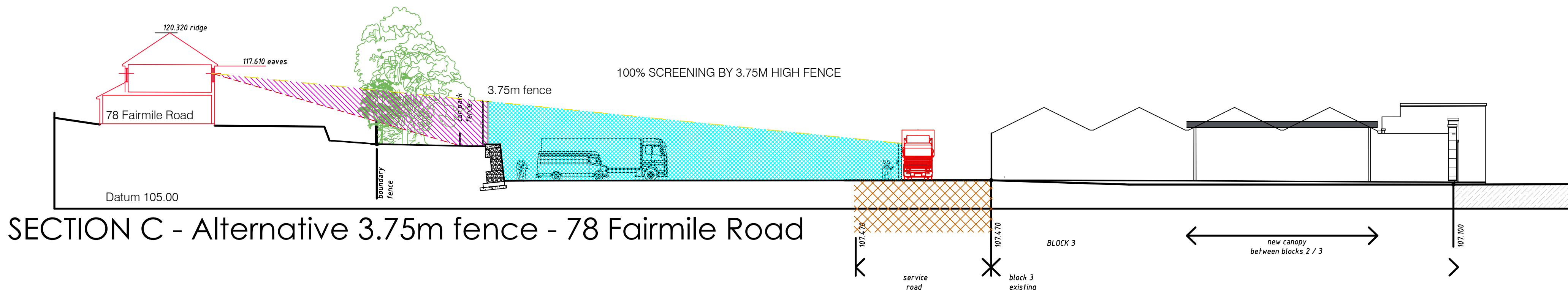
A1



A 12.04.23 rev. | date | acoustic fence height increased PS SC
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Tel: 0121 550 0359 Fax: 0121 550 8088
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Site Sections across Yard = Sheet 1 of 2
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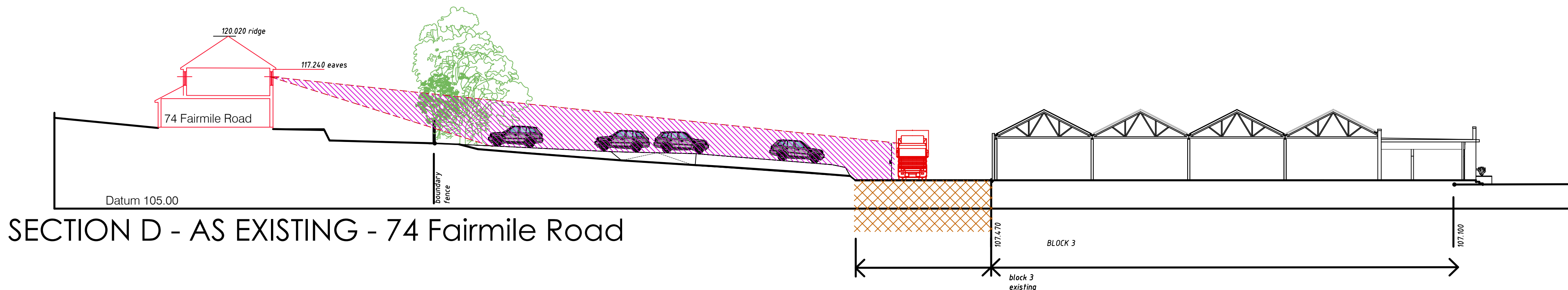


SECTION C - AS EXISTING - 78 Fairmile Road

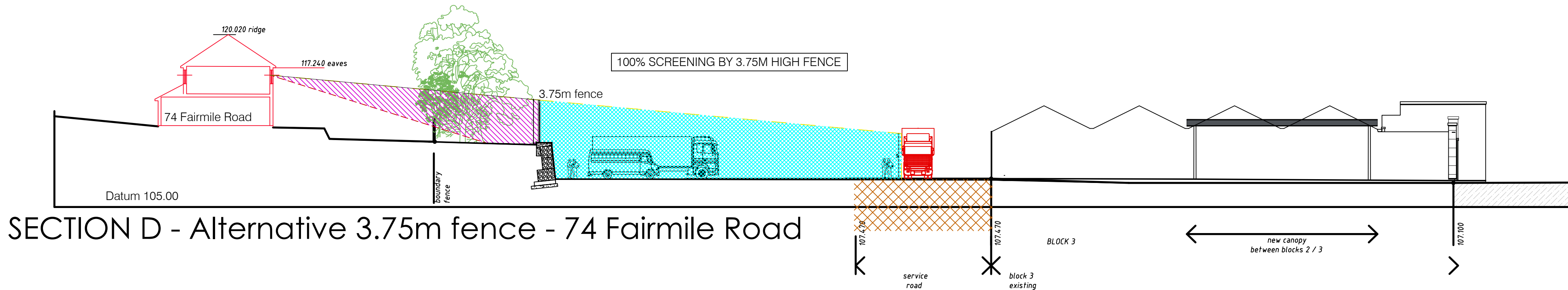


SECTION C - Alternative 3.75m fence - 78 Fairmile Road

LINE OF SIGHT
EXISTING SERVICE ROUTE
SCREENED ZONE - ALTERNATIVE 3.75M FENCE



SECTION D - AS EXISTING - 74 Fairmile Road



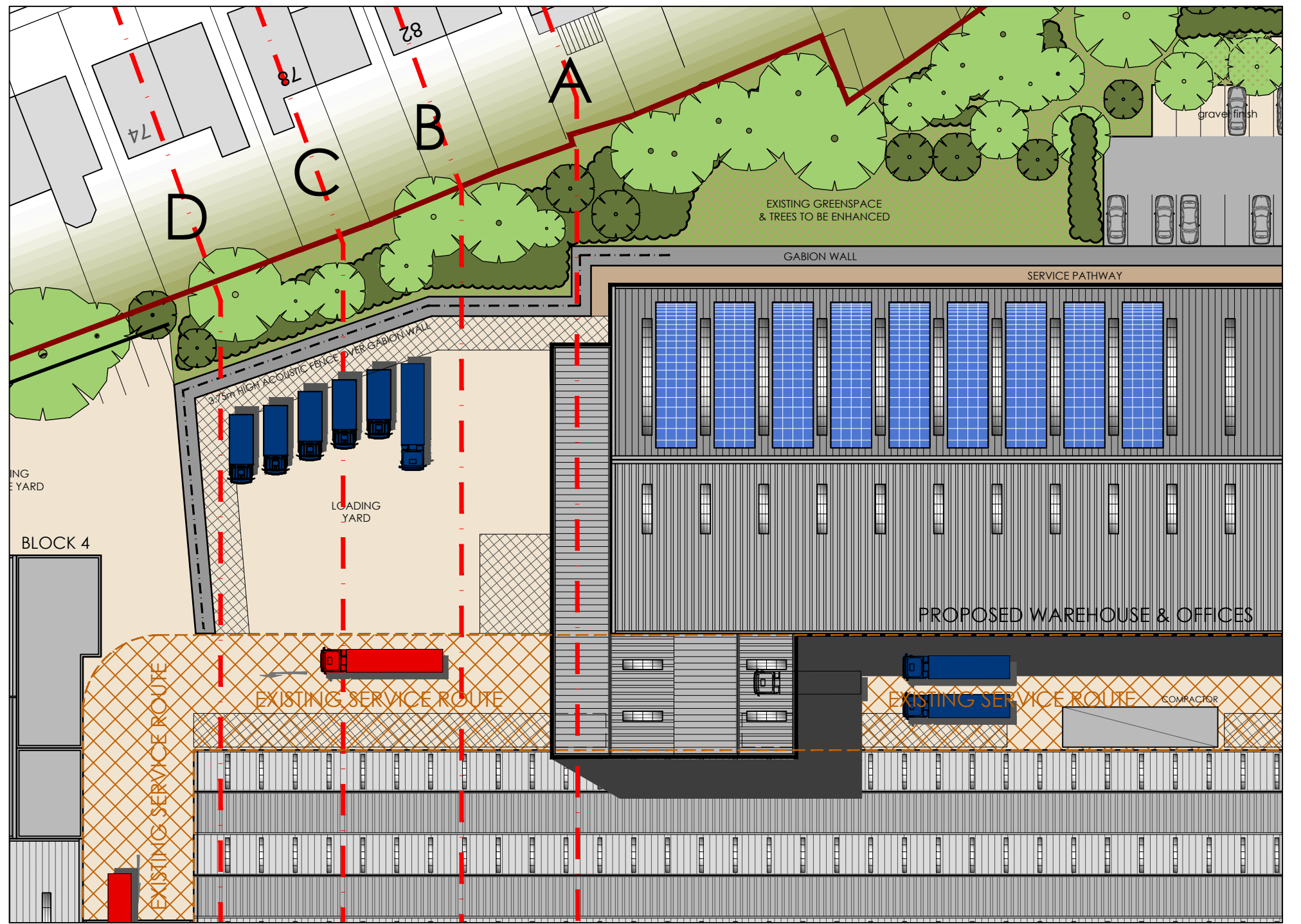
SECTION D - Alternative 3.75m fence - 74 Fairmile Road

Note

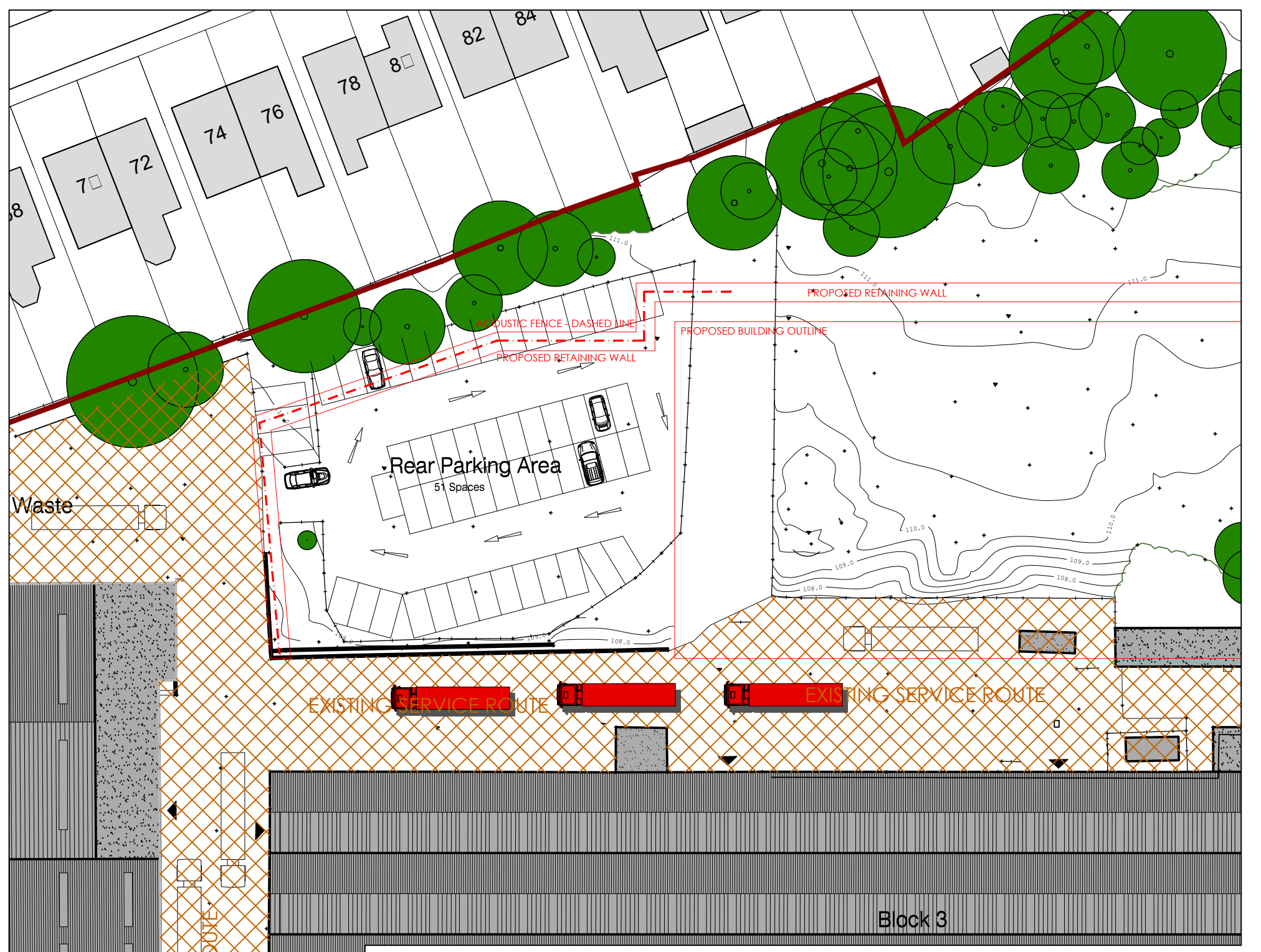
All references to the specification of any product, material or system used in the proposed construction, repair, treatment and refurbishment of any building or structure detailed on this drawing, in respect of the performance of combustibility, fire resistance or fire protection shall be fully in accordance with manufacturer's or supplier's specifications and recommendations and in accordance with current applicable regulations. Where specified refer to fire engineer consultant's specific details where appointed.

No reliance shall be placed on such details on this drawing.

A1



Proposed Service route & Yard 1:500



Existing Service routes 1:500

A 12.04.23 acoustic fence height increased PS SC

rev. | date | drawn | chk'd

Gould Singleton Architects
Earls Way, Halesowen, West Midlands, B63 3HR
Tel: 0121 550 0359 Fax: 0121 550 8088
e-mail: info@gould-singleton.co.uk

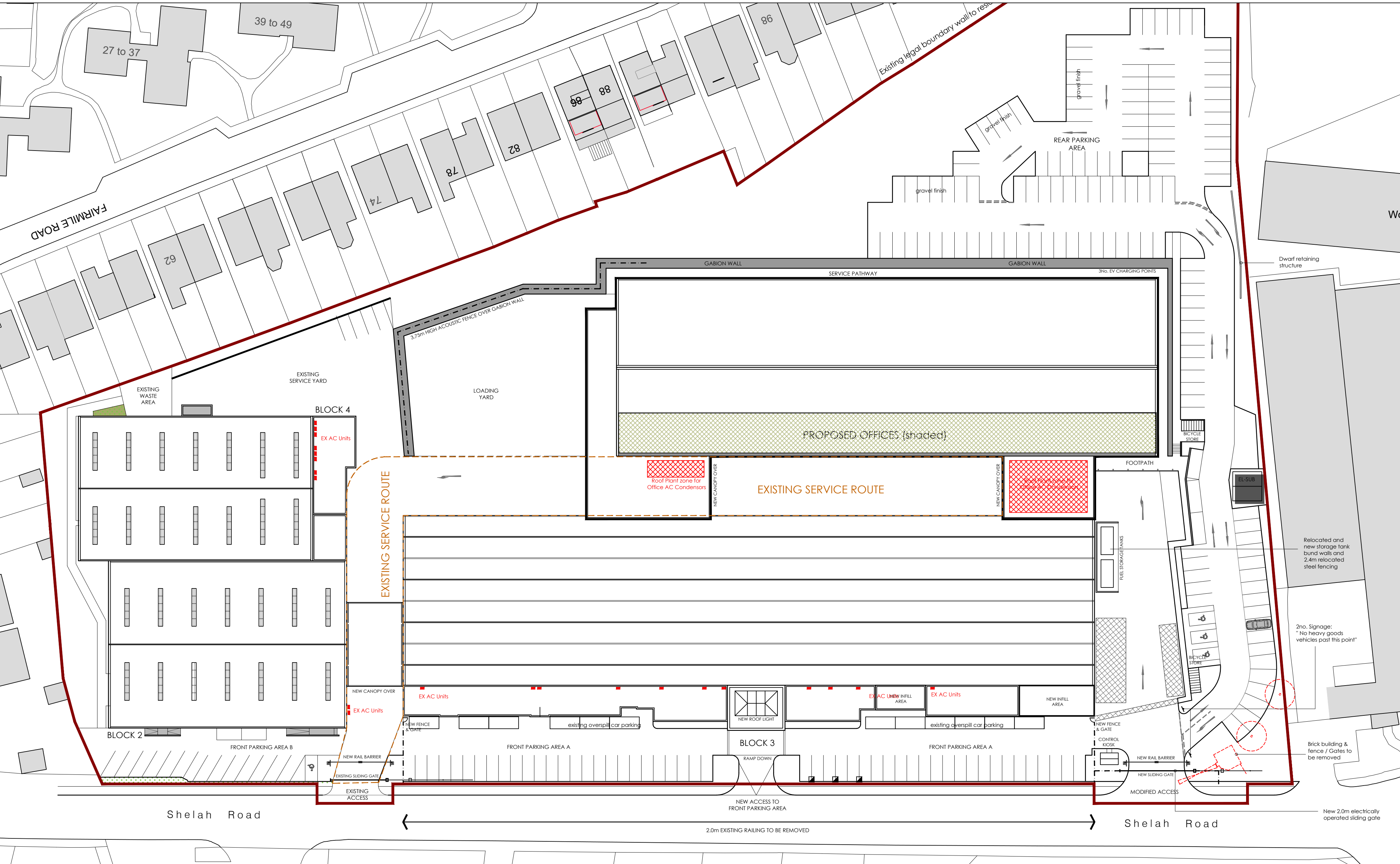
1:250 Hayley Group Limited
Shelah Road
Halesowen
B63 3XL

6/3/23

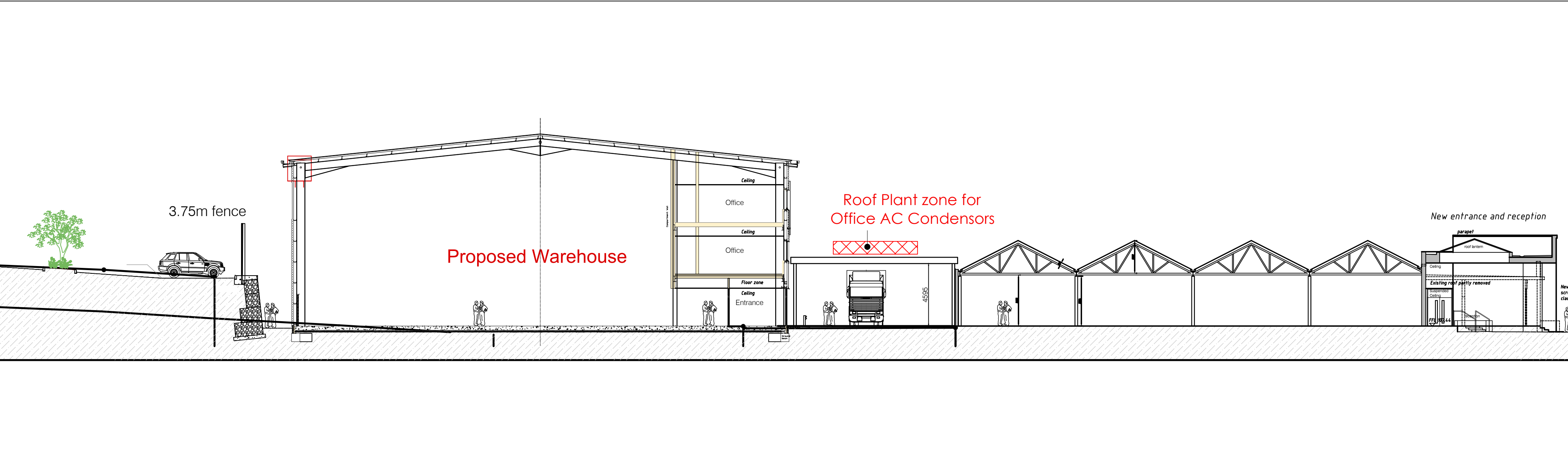
SDC GSA Site Sections across Yard = Sheet 2 of 2

number drawn 21-1883/25A

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Proposed Service route & Yard 1:500



Section showing proposed location of AC Units to Canopies

..\\Users\\stephen.cox\\Pictures\\hayl

Suggested Plant locations

A	12.04.23	acoustic fence high increased	PS	SC
rev.	date		drawn	chk'd

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scales	1:500	Hayley Group Limited Shelah Road Halesowen B63 3XL
date	6/3/23	
drawn	SDC	checked GSA
number	21-1883/26A	AC Condensor Locations to New Offices

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